

ALLOWABLE RANGES TABLE

FOR THIS PROJECT, CROSS SLOPES THAT ARE ADJUSTED TO "BEST FIT" EXISTING PAVEMENT SLOPES ARE SUBJECT TO THE FOLLOWING LIMITS:

A. NORMAL CROWN

SECTION WITH GRADES 0.5% OR GREATER	SECTION WITH GRADES LESS THAN 0.5%
0.0150 FT/FT - MINIMUM	0.0156 FT/FT - MINIMUM
0.0208 FT/FT - DESIRABLE	0.0208 FT/FT - DESIRABLE
0.0250 FT/FT - MAXIMUM	0.0300 FT/FT - MAXIMUM

B. SUPERELEVATION RATE

S.E. RATE SHOWN ON PLANS OR SE RATE EXISTING IN FIELD, WHICHEVER IS GREATER.

C. SUPERELEVATION TRANSITION LENGTH (LENGTH FROM FLAT POINT TO FULL SE)

RATE OF CHANGE	CORRESPONDING DIFFERENCE IN GRADE BETWEEN PIVOT POINT AND EDGE OF PAVEMENT
MINIMUM 1:150	0.67%
DESIRABLE 1:200	0.50%
MAXIMUM 1:300	0.33%

LENGTH SHALL BE SET TO AVOID CREATING A FLAT GUTTER GRADE ON LOW SIDE AND TO AVOID FLAT CROSS SLOPES AT OR NEAR THE LOW POINT OF VERTICAL CURVES.

D. POSITIONING OF SUPERELEVATION TRANSITION LENGTH ON SIMPLE CURVES

- 50% OF TRANSITION INSIDE CURVE - MAXIMUM
- 33% OF TRANSITION INSIDE CURVE - DESIRABLE
- 20% OF TRANSITION INSIDE CURVE - MINIMUM

NOTE: CROWN WIPE-OUT SHALL BE AT THE SAME RATE AS THE SE TRANSITION.

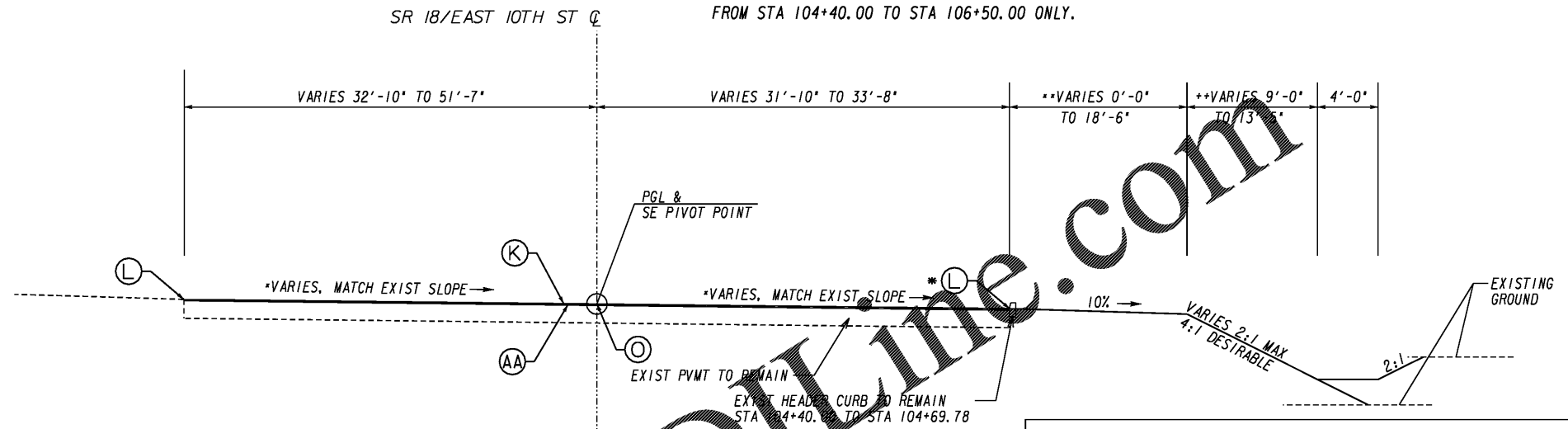
E. SMOOTHING OF BREAKS IN EDGE PROFILE AT BEGIN AND END OF TRANSITION SHALL BE ACCOMPLISHED BY VERTICAL CURVE WITH A MINIMUM LENGTH (1 IN FEET) EQUAL TO THE SPEED DESIGN (1 IN MPH).

SLOPE CONTROLS FILL & CUT	
SLOPE	HIGH FROM SHOULDER PT
4:1	0' TO 6'
2:1	6' AND OVER

*GUARDRAIL IS REQUIRED ON FILL SECTION

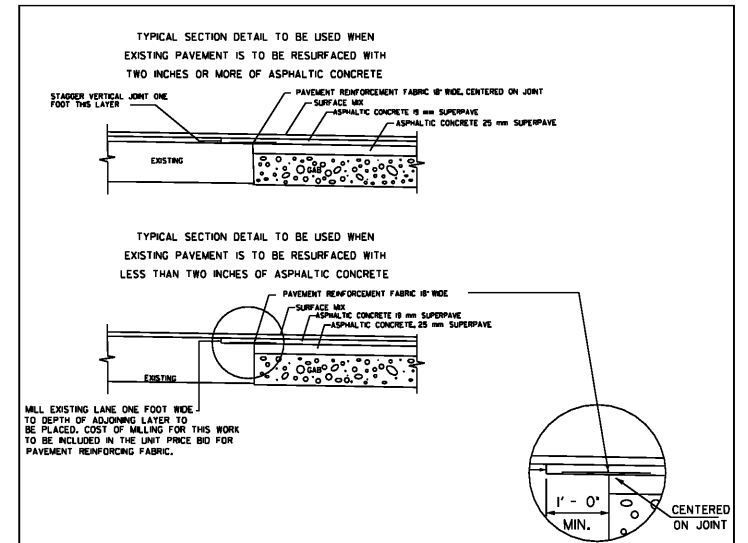
*SEE ALLOWABLE RANGES TABLE FOR MINIMUM AND MAXIMUM SLOPE ALLOWED ON THE TRAVEL LANES.

*NOTE: SAFETY EDGE TREATMENT BEGINS AT 104+69.78 RT
**NOTE: SHOULDER GRADING TO BE CONSTRUCTED FROM STA 104+40.00 TO STA 106+50.00 ONLY.



TYPICAL SECTION NO. 1
MILLING, INLAY, AND SHOULDER GRADING
SR 18/EAST 10TH ST
STA 104+40.00 TO STA 106+50.00

**NOTE: SPECIAL 4' FLAT BOTTOM DITCH IS FROM STA 106+00.00 TO STA 107+50.00



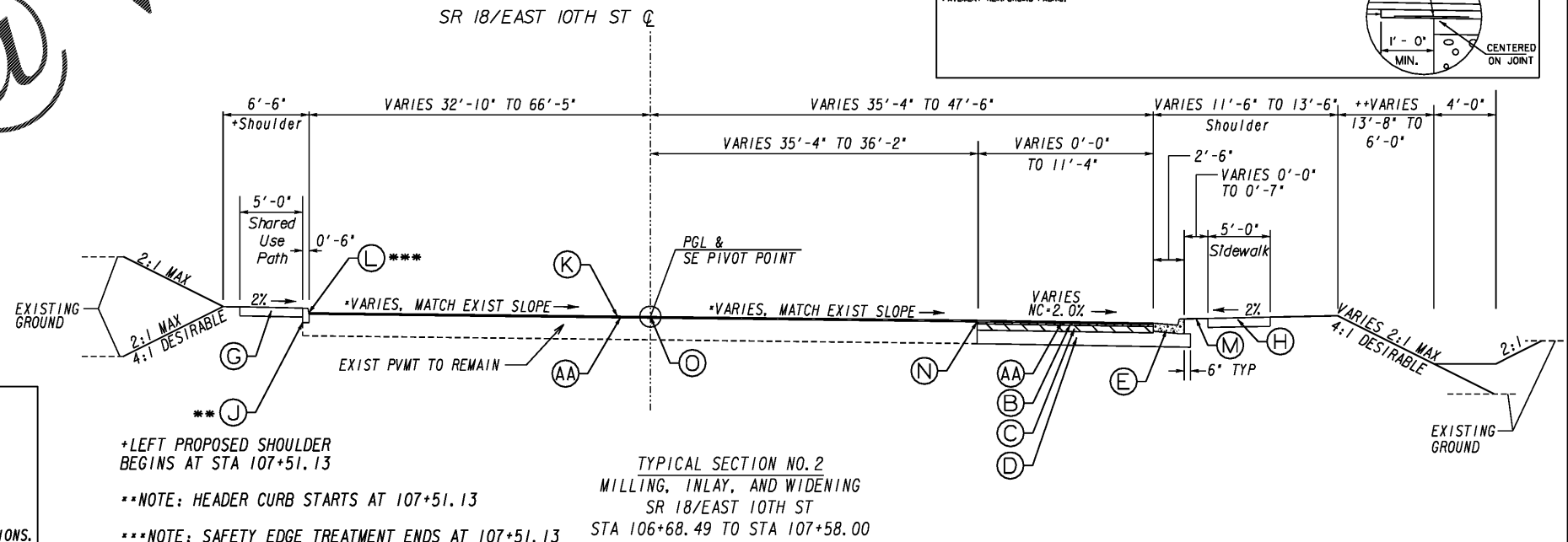
MILL EXISTING LANE ONE FOOT WIDE TO DEPTH OF ADJOINING LAYER TO BE PLACED. COST OF MILLING FOR THIS WORK TO BE INCLUDED IN THE UNIT PRICE BID FOR PAVEMENT REINFORCING FABRIC.

LEGEND

- (AA) RECYCLED ASPH CONC 12.5 MM SUPERPAVE, GP 2 ONLY, INCL POLYMER-MODIFIED BITUM MATL & H LIME (165 LBS/SY)
- (A) RECYCLED ASPH CONC 12.5 MM SUPERPAVE, GP 2 ONLY, INCL BITUM MATL & H LIME (165 LBS/SY)
- (B) RECYCLED ASPH CONC 19 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (200 LBS/SY)
- (C) RECYCLED ASPH CONC 25 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (800 LBS/SY)
- (D) GR AGGR BASE CRS, 12 INCH, INCL MATL
- (E) CONC CURB & GUTTER, 6 IN X 30 IN, TP 2
- (F) CONC CURB & GUTTER, 6 IN X 30 IN, TP 7
- (G) CONC SIDEWALK, 6 IN
- (H) CONC SIDEWALK, 4 IN
- (I) CONCRETE MEDIAN, 4 IN
- (J) CONCRETE HEADER CURB, 6 IN, TP 2
- (K) MILL ASPH CONC PVT, 1 1/2 IN DEPTH
- (L) PAVEMENT EDGE TREATMENT PER SA DETAIL P-7
- (M) PLAIN STAMPED CONCRETE
- (N) PAVEMENT REINFORCEMENT FABRIC, TP 2, 18'
- (O) CENTERLINE RUMBLE STRIPS

NOTES:

- SEE GEORGIA STANDARDS 4000 SERIES FOR GUARDRAIL SHOULDER INFORMATION, AND GEORGIA DETAIL S-4 FOR SURFACING TO THE FACE OF THE GUARDRAIL.
- SEE ROADWAY PLANS FOR CROSS SLOPE TRANSITIONS.
- SEE ROADWAY PLANS FOR LOCATION OF GUARDRAIL.
- SEE ROADWAY PLANS FOR PAVEMENT WIDTH TRANSITIONS.
- SEE CROSS SECTIONS FOR SLOPE INFORMATION & LOCATIONS.



TYPICAL SECTION NO. 2
MILLING, INLAY, AND WIDENING
SR 18/EAST 10TH ST
STA 106+68.49 TO STA 107+58.00

*LEFT PROPOSED SHOULDER BEGINS AT STA 107+51.13

**NOTE: HEADER CURB STARTS AT 107+51.13

***NOTE: SAFETY EDGE TREATMENT ENDS AT 107+51.13

INFRASTRUCTURE CONSULTING & ENGINEERING
4940 PEACHTREE INDUSTRIAL BLVD., SUITE 310
NORCROSS, GEORGIA 30071

NOT TO SCALE

REVISION DATES

**TYPICAL SECTIONS
SR 18 BRIDGE REPLACEMENT @ LONG CAME CREEK
TROUP COUNTY**

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	05-0001
CORRECTED:	DATE:	
VERIFIED:	DATE:	