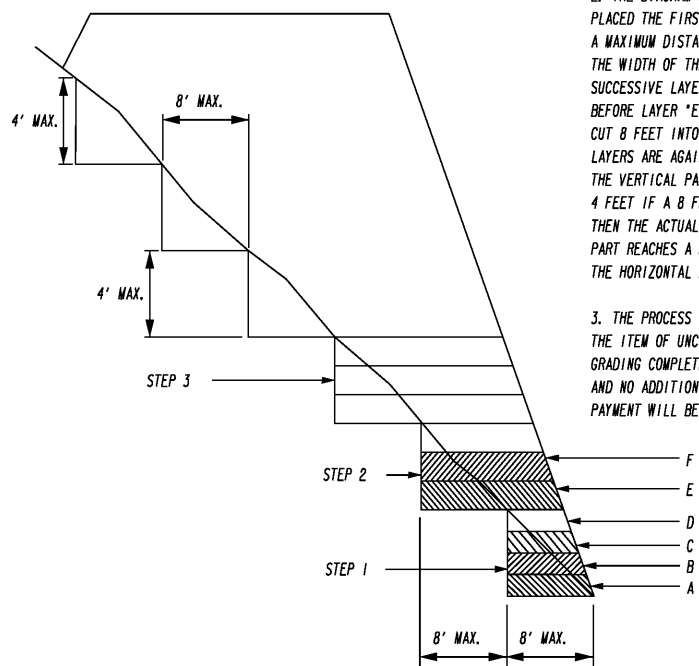


1. WHERE THE EMBANKMENT IS TO BE PLACED ON A HILLSIDE OR ANOTHER EXISTING EMBANKMENT HAVING A SLOPE OF 3 TO 1 OR STEEPER, THE FOUNDATION MUST BE BENCHING WHILE THE EMBANKMENT IS BEING MADE.
(SEE DIAGRAM AT LEFT.)

2. THE DIAGRAM SHOWS THAT BEFORE LAYER "A" IS PLACED THE FIRST STEP IS TO (1) CUT INTO THE SLOPE A MAXIMUM DISTANCE OF ABOUT 8 FEET (ABOUT THE WIDTH OF THE TYPICAL D-8 BULLDOZER BLADE). SUCCESSIVE LAYERS B, C, AND D ARE THEN PLACED BEFORE LAYER "E" IS PLACED, THE SECOND STEP IS CUT 8 FEET INTO THE SLOPE AND SUCCESSIVE LAYERS ARE AGAIN PLACED. IF IT IS ANTICIPATED THAT THE VERTICAL PART OF THE STEP WILL EXCEED 4 FEET IF A 8 FEET HORIZONTAL CUT IS MADE, THEN THE ACTUAL CUT STOPS WHEN THE VERTICAL PART REACHES A MAXIMUM OF 4 FEET ALLOWING THE HORIZONTAL DISTANCE TO VARY.

3. THE PROCESS OF BENCHING IS CONSIDERED INCIDENTAL TO THE ITEM OF UNCLASSIFIED EXCAVATION AND BORROW OR GRADING COMPLETE IN CONSTRUCTION OF THE EMBANKMENT AND NO ADDITIONAL MEASUREMENT OF QUANTITY OR PAYMENT WILL BE MADE FOR BENCHING.



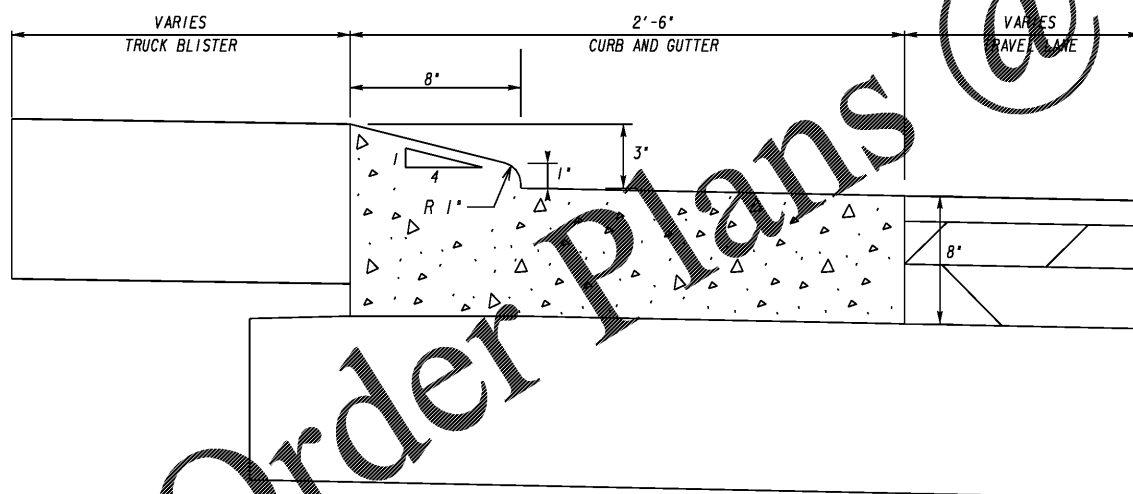
BENCHING DETAIL

Revised 9/29/08

4.5.28

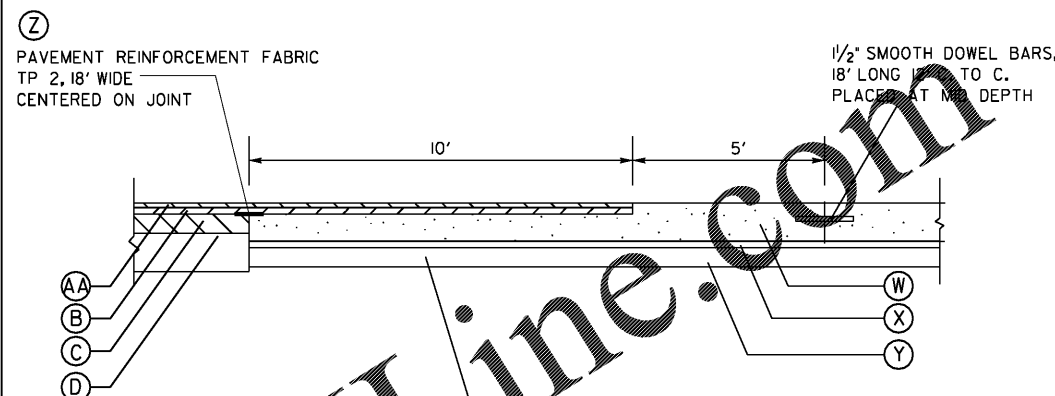
NO SCALE

DETAIL A
CONC CURB & GUTTER, 8 IN X 30 IN, TP 9
FOR TRUCK APRON AND BLISTER



1. TRUCK APRON/BLISTER CURB AND GUTTER SHALL BE MEASURED AND PAID FOR AS PAY ITEM 441-6743

JOINTED PLAIN CONCRETE PAVEMENT TO ASPHALT PAVEMENT TRANSITION



NOTE: THE REDUCED THICKNESS CONCRETE SLAB SHALL BE PAID FOR AS PLAIN PC CONCRETE PAVEMENT. NO ADDITIONAL COMPENSATION WILL BE MADE FOR THE REDUCED DEPTH.

- (AA) RECYCLED ASPH CONC 12.5MM SUPERPAVE, GP 2 ONLY, INCL POLYMER-MODIFIED BITUM MATL & H LIME, 165 LB/SY
- (A) RECYCLED ASPH CONC 12.5MM SUPERPAVE, GP 2 ONLY, INCL BITUM MATL & H LIME, 165 LB/SY
- (B) RECYCLED ASPH CONC 19 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME, 220 LB/SY
- (C) RECYCLED ASPH CONC 25 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME, 880 LB/SY
- (Z) RECYCLED ASPH CONC 25 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME, 660 LB/SY
- (3) RECYCLED ASPH CONC 25 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME, 770 LB/SY
- (D) GR AGGR BASE CRS, 12 INCH, INCL MATL
- (E) CONC CURB & GUTTER, 8 IN X 30 IN, TP2
- (EB) CONC CURB & GUTTER, 8 IN X 30 IN, TP9
- (F) CONC CURB & GUTTER, 8 IN X 30 IN, TP7
- (G) CONC CURB & GUTTER, 8 IN X 30 IN, TP 9, SEE DETAIL A (SHEET 05-0013)
- (H) CONC SIDEWALK, 8 IN
- (I) CONCRETE HEADER CURB, 6 IN, TP 7
- (K) MILL ASPH CONC PVMT, VARIABLE DEPTH
- (L) PAVEMENT EDGE TREATMENT FOR ASPHALT AND CONCRETE PAVEMENT, GDOT DTL P-7
- (M) CONCRETE MEDIAN, 7 1/2 IN
- (N) PLAIN PC CONC PVMT, CL 3 CONC, 10 INCH TK COLORED AND STAMPED (FEDERAL COLOR *31136 INSIGNIA RED)
- (O) SOD
- (P) PERMANENT GRASSING
- (R) INDENTATION EDGE LINE RUMBLE STRIPS- GROUND-IN-PLACE (SKIP)
- (S) INDENTATION CENTERLINE RUMBLE STRIPS- GROUND-IN-PLACE (CONT)
- (W) PLAIN PC CONC PVMT, CL 3 CONC, 11 IN THK
- (X) RECYCLED ASPH CONC 19 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME, 330 LB/SY
- (Y) GR AGGR BASE CRS, 8 INCH, INCL MATL
- (Z) PAVEMENT REINFORCEMENT FABRIC, TP 2, 18 IN WIDTH
 - * SEE CROSS SECTIONS FOR SLOPES
 - ** SEE PLANS AND CROSS SECTIONS FOR S.E.
 - *** 6" HEIGHT TRANSITION TO 3" HEIGHT WHERE THERE IS TRUCK BLISTER
 - **** SEE ROADWAY PLANS FOR GUARDRAIL LOCATIONS

Order Plans @

Kimley»Horn

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Suite 350, 3930 East Jones Bridge Road
Peachtree Corners, Georgia 30092

NOT TO SCALE

REVISION DATES

NO.	DATE	DESCRIPTION

TYPICAL SECTIONS
I-85 @ SR 18 & SR 18 @ SR 103
JOINTED PLAIN CONC TO ASPHALT TRANSITION

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	05-0013
CORRECTED:	DATE:	
VERIFIED:	DATE:	