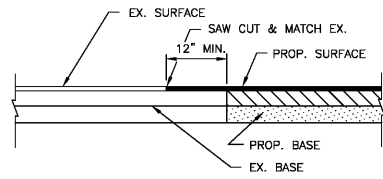
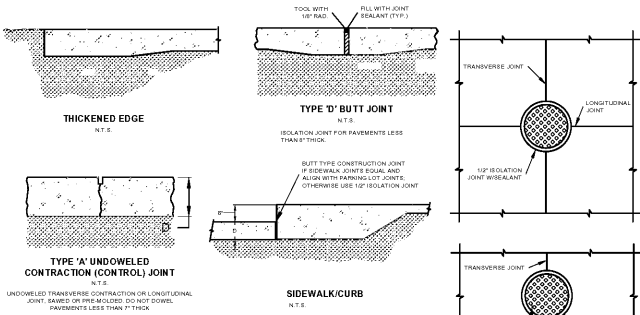


TIE-IN OF PROPOSED PAVEMENT TO EXISTING
NTS



RECOMMENDED MAX. JOINT SPACINGS	RECOMMENDED MAXIMUM JOINT SPACING (FEET)
3.5 (FOR WHITE TOPPING ONLY)	6
4.0	10
4.5	10
5.0	12
5.5	12
6.0	15
OVER 6.0	15

NOTE: CONTRACTOR TO SUBMIT A CONCRETE JOINT LAYOUT TO ENGINEER PRIOR TO CONSTRUCTION.



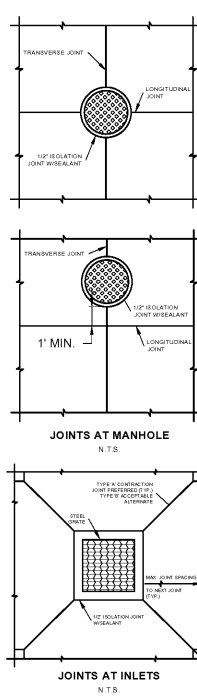
GENERAL NOTES:

- USE ACI OR FCMA CERTIFIED FINISHER
- USE ACI 308-01 GUIDE FOR DESIGN AND CONSTRUCTION OF CONCRETE PARKING LOTS
- USE ACI 308-14 STANDARD SPECIFICATION FOR PLAIN CONCRETE PARKING LOTS
- ALL CONCRETE USED IN PARKING LOT SHALL HAVE A COMPRESSIVE STRENGTH OF 4000 PSI AT 28 DAYS
- PREPARE THE SUBGRADE IN ACCORDANCE WITH THE GEOTECHNICAL ENGINEER'S RECOMMENDATIONS FOR ROAD PAVEMENTS. SUBGRADE SOIL DENSITY TESTING MUST BE COMPLETED AND VERIFIED BY THE GEOTECHNICAL ENGINEER PRIOR TO CONCRETE PLACEMENT
- IMPORTED SOIL USE FOR BACKFILL SHOULD BE FREE OF HEAVY CLAY, SILTS, STONES, PLANT ROOT OR OTHER FOREIGN MATERIAL GREATER THAN 1/2" IN DIAMETER IN ORDER TO ACHIEVE ADEQUATE COMPACTION AROUND ANY FIXED OBJECT IN GROUND. ALTERNATE WILL BE TO USE FLOWABLE FILL
- LAYOUT CONTROL JOINT BY STARTING WITH ANY DRAINAGE INLET WITHIN THE PAVEMENT SECTION AND WORK TOWARD EDGE OF PAVEMENT
- KEEP ALL JOINTS CONTINUOUS
- CONTROL JOINTS SHALL BE FORMED OR SAWED WITHIN 12 HOURS FROM TIME OF PLACEMENT. A SIDEWALK SPACING SHALL BE SAME AS WIDTH OF PAVEMENT AND LESS THAN 5 FEET IN LENGTH
- PAVEMENT MAXIMUM SPACING SHALL BE 2.5 TIMES THICKNESS IN UNIT OF FEET AND LESS THAN 15 FEET IN LENGTH (E.G. 14 INCH SPACING AT 20 FT)
- CURE CONCRETE IMMEDIATELY AFTER FINISHING OPERATION IS COMPLETED BY USING ONE OF THE FOLLOWING METHODS: WATER, PNEUMATIC WATER-BASED CURING COMPOUND OR VISQUEEN AND BURLAP

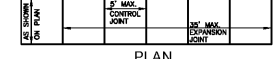
CURBS:

- ALL CURBS SHALL BE CONSTRUCTED OF CONCRETE THAT WILL OBTAIN A MINIMUM COMPRESSIVE STRENGTH OF 4000 PSI (MIN) AT 28 DAYS
- ALL CONCRETE CURBS SHALL BE SPACED WITH A FULL DEPTH 1/2" WIDTH ISOLATION JOINT MATERIAL PRIOR TO PLACEMENT OF ADJACENT CONCRETE PAVEMENT
- THERE SHALL BE CONTROL JOINTS EITHER TOOL OR SAW CUT MADE IN PAVEMENT JOINTS UNLESS OTHERWISE SPECIFIED. JOINTS SHALL BE FORMED WITHIN 12 HOURS OF PLACEMENT
- ALL CURB BARS THAT DO NOT TIE INTO OTHER FACILITIES SHALL TRANSITION DOWN TO PAVEMENT GRADE IN 24 INCHES
- CONSTRUCTION JOINTS SHALL BE TIED WITH A 4" X 4" BAR EXTENDED 6 INCHES INTO EACH CURB SECTION AND SHALL BE SPACED WITH A FULL DEPTH 1/2" WIDTH ISOLATION JOINT MATERIAL

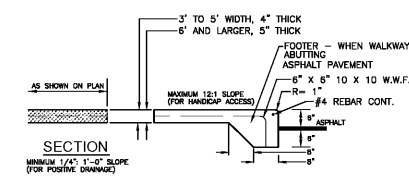
CONCRETE JOINT DETAILS
NTS



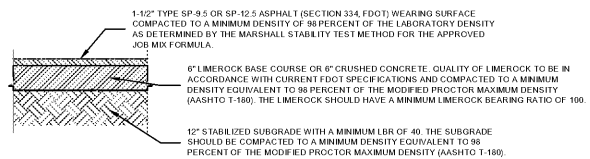
ON-SITE RAISED SIDEWALK DETAIL
NTS



NOTE: ALL CONCRETE SHALL BE 4,000 PSI CONCRETE. EXPANSION JOINTS SHALL BE AT P.C.'S AND P.T.'S AND AT 35 FOOT INTERVALS ON STRAIGHT RUNS. DUMMY JOINTS SHALL BE AT LEAST 5 FEET APART.

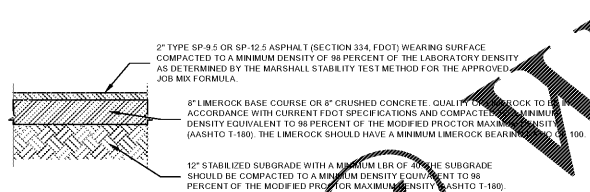


ON-SITE LIGHT DUTY ASPHALT PAVEMENT SECTION (FLEXIBLE)
NTS



- CONTRACTOR MUST INSTALL PAVEMENT AS SMOOTH AS POSSIBLE & KEEP IT FREE OF ANY MARKS, SCRAPES, GOUGES, TIRE MARKS, ETC., THAT MIGHT BE CAUSED BY CONSTRUCTION TRAFFIC & EQUIPMENT.
- CONTRACTOR SHALL LIMIT PLACING THE ASPHALT PAVEMENT COURSE UNTIL ALL OTHER CONSTRUCTION IS COMPLETE & NO OTHER CONSTRUCTION TRAFFIC IS ANTICIPATED.

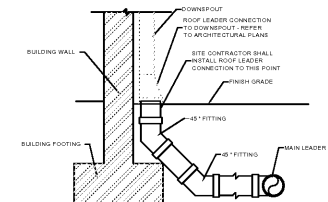
ON-SITE HEAVY DUTY ASPHALT PAVEMENT SECTION (FLEXIBLE)
NTS



- CONTRACTOR MUST INSTALL PAVEMENT AS SMOOTH AS POSSIBLE & KEEP IT FREE OF ANY MARKS, SCRAPES, GOUGES, TIRE MARKS, ETC., THAT MIGHT BE CAUSED BY CONSTRUCTION TRAFFIC & EQUIPMENT.
- CONTRACTOR SHALL LIMIT PLACING THE ASPHALT PAVEMENT COURSE UNTIL ALL OTHER CONSTRUCTION IS COMPLETE & NO OTHER CONSTRUCTION TRAFFIC IS ANTICIPATED.

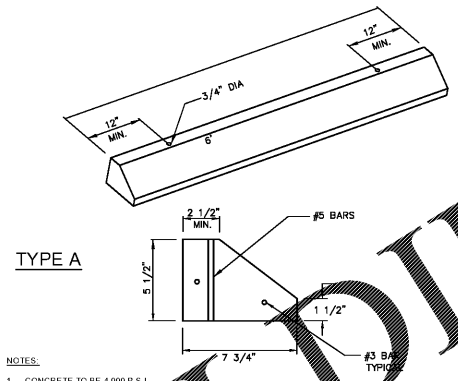
ON-SITE HEAVY DUTY ASPHALT PAVEMENT SECTION (FLEXIBLE)
NTS

ROOF LEADER CONNECTION DETAIL
NTS



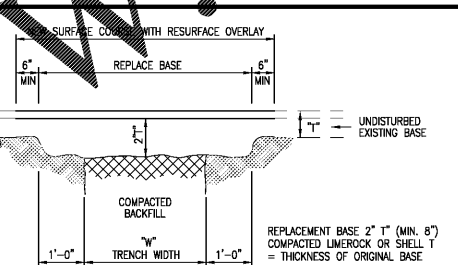
- NOTES:
- REFER TO SITE PLANS FOR PIPE SIZES AND SPECIFICATIONS
 - SITING CONTRACTOR IS RESPONSIBLE FOR ALL LABOR AND MATERIAL UP TO THE DOWNSPOUT CONNECTION.
 - ACTUAL DOWNSPOUT CONNECTION SHALL BE SUPPLIED AND INSTALLED BY BUILDING CONTRACTOR.

WHEEL STOP DETAIL
NTS



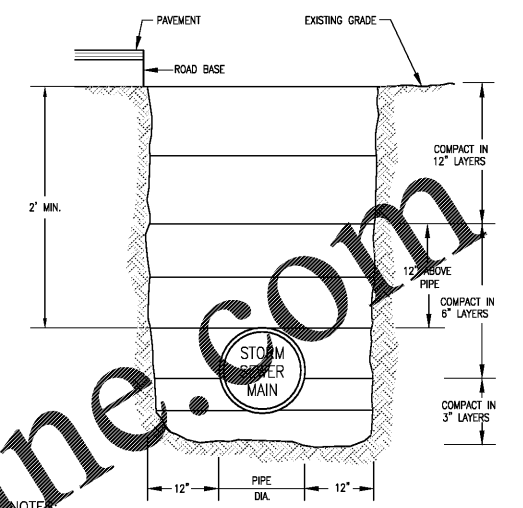
- NOTES:
- CONCRETE TO BE 4000 P.S.I.
 - WHEEL STOPS TO BE SECURED TO FLOOR WITH TWO 1/2" DIA. #5 BARS
 - RECYCLED PLASTIC WHEEL STOPS MAY BE USED IN LIEU OF CONCRETE
 - DIMENSIONS OF WHEEL STOPS MAY VARY FROM THE ABOVE SPECIFICATIONS IF APPROVED BY ENGINEER

PAVEMENT REPLACEMENT DETAIL
NTS



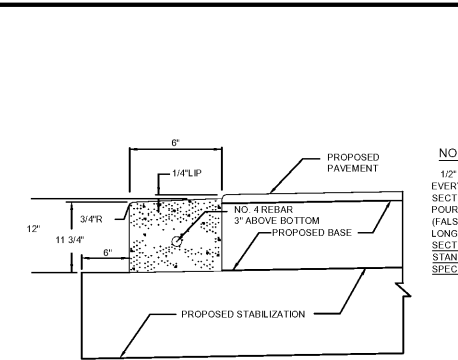
- NOTES:
- CONTRACTOR TO RESURFACE TO ALL VALVES WITHIN EXISTING ROADWAY AND NOT COVER.
 - ALL GRASSED AREAS DISTURBED WITHIN THE RIGHT-OF-WAY SHALL BE SOODED TO ORIGINAL OR BETTER CONDITION.
 - ALL GEOTECHNICAL TESTING OF WORK WITHIN THE RIGHT-OF-WAY SHALL BE PER CITY SPECIFICATIONS.

TRENCH BACKFILL DETAIL
NTS



- NOTES:
- TRENCHES LOCATED UNDER PAVEMENT SHALL BE COMPACTED TO 100% OF MAXIMUM DENSITY BY A.A.S.H.T.O. T-99, METHOD C.
 - TRENCHES NOT LOCATED UNDER PAVEMENT SHALL BE COMPACTED TO A FIRMNESS EQUAL TO THAT OF THE SOIL ADJACENT TO THE TRENCH.
 - REPLACEMENT OF PAVED SURFACES SHALL BE MADE IN ACCORDANCE TO APPLICABLE LOCAL REGULATIONS.

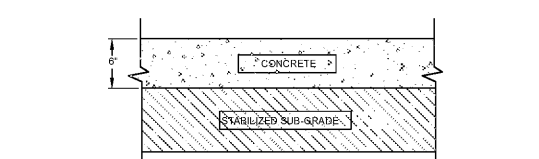
RIBBON CURB DETAIL
NTS



NOTE: 1/2" EXPANSION JOINT EVERY 500' BETWEEN CURB SECTIONS. AT RADII END OF POUR AND STRUCTURES. (FALSE JOINTS EVERY 10', LONGITUDINALLY SEE SECTION 520 OF THE FDOT STANDARD SPECIFICATIONS)

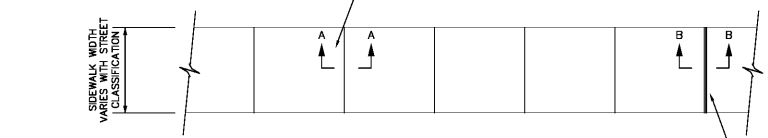
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TYP. CONCRETE PAVEMENT SECTION
NTS

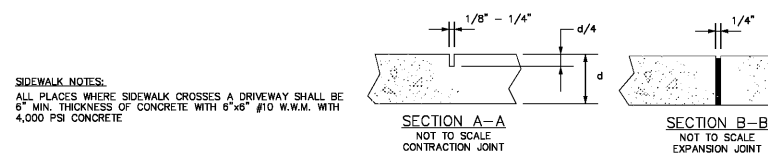


- CONCRETE: CONCRETE PAD SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 4,000 PSI AFTER 28 DAYS OF LABORATORY CURING PER ASTM C-31 AND REINFORCED WITH FIBER MESH AT 1.5 LBS PER CUBIC YARD OF CONCRETE.
- SUB-GRADE:
- STABILIZED SUBGRADE SOILS SHALL HAVE A MINIMUM LBR OF 40 AND COMPACTED TO AT LEAST 98 PERCENT OF THE MODIFIED PROCTOR MAXIMUM DRY DENSITY (ASTM D-1557).
 - THE SURFACE OF THE SUBGRADE SOILS MUST BE SMOOTH AND ANY ANOMALIES OR WHEEL RUTTING CORRECTED PRIOR TO PLACEMENT OF THE CONCRETE.
 - THE SUBGRADE SOILS MUST BE MOISTENED PRIOR TO PLACEMENT OF THE CONCRETE.
 - CONCRETE PAVEMENT THICKNESS SHOULD BE UNIFORM THROUGHOUT, WITH THE EXCEPTION OF THICKENED EDGES (CURB OR FOOTING).
 - THE BOTTOM OF THE PAVEMENT SHOULD BE SEPARATED FROM THE ESTIMATED SHAWL BY AT LEAST 18 IN.

6" THICK SLAB USES 1 1/2" DEEP JOINT.
4" THICK SLAB USES 1" DEEP JOINT.
EXAMPLE:
SLAB THICKNESS DIVIDED BY 4 (d/4 = DEPTH OF JOINT).
SHALL BE 1/8" TO 1/4" WIDE. DEPTH OF JOINT SHALL BE SPACED AT INTERVALS EQUAL TO SIDEWALK WIDTH. JOINTS SAWED OR PREFORMED. TRANSVERSE JOINTS SHALL BE TRANSVERSE OR LONGITUDINAL CONTRACTION JOINTS MAY BE CONTRACTION JOINTS.



EXPANSION JOINTS: EXPANSION JOINTS ARE TO BE USED BETWEEN SIDEWALKS AND CURB, DRIVEWAYS, SIDEWALK INTERSECTIONS OR AT FIXED OBJECTS. ALL JOINTS SHALL BE 1/4" THICK, AND SHALL BE FORMED WITH PREFORMED EXPANSION JOINT MATERIAL PER F.D.O.T. SPEC. #932-1, AND SHALL MEET AASHTO M-153 OR 213



TYPICAL CONCRETE SIDEWALK DETAILS
NTS

SIDEWALK NOTES:
ALL PLACES WHERE SIDEWALK CROSSES A DRIVEWAY SHALL BE 6" MIN. THICKNESS OF CONCRETE WITH 6"x6" #10 W.W.M. WITH 4,000 PSI CONCRETE

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PERMIT SET

MATTHEW D. CAMPO
LICENSE
No. 53988
STATE OF FLORIDA
PROFESSIONAL ENGINEER

Matthew D. Campo 53988
NAME SEAL NO. DATE

FGA PROJECT NUMBER
19048

ISSUE DATE
04-15-2020

REVISIONS	
NO.	NOTES

SHEET NAME

DETAILS

SHEET NUMBER
C-6