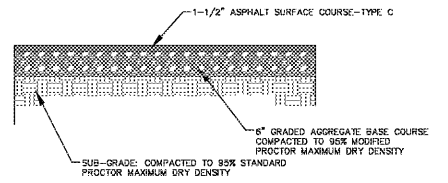
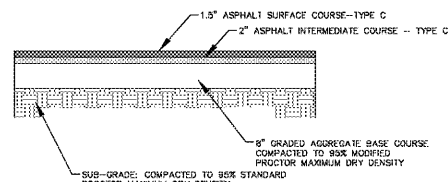


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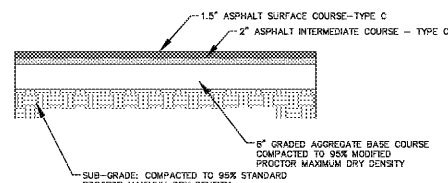
NOTES:  
1. PAVEMENT DESIGN PER GEOTECHNICAL REPORT PREPARED BY TERRACON DATED OCTOBER 10, 2016. REFER TO REPORT FOR SPECIFIC INFORMATION.  
2. LIGHT DUTY PAVEMENT TO BE INSTALLED AT PARKING SPACES AS SHOWN ON LAYOUT PLAN.  
3. ASPHALT CONCRETE AND ALL CRUSHED STONE MATERIALS SHOULD CONFORM TO THE SCDOT STANDARD SPECIFICATIONS.

ASPHALT PAVEMENT SECTION DETAIL (LIGHT DUTY)  
NOT TO SCALE



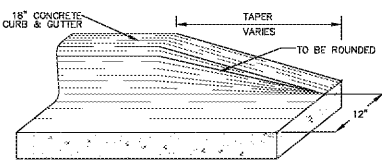
NOTES:  
1. PAVEMENT DESIGN PER GEOTECHNICAL REPORT PREPARED BY TERRACON DATED OCTOBER 10, 2016. REFER TO REPORT FOR SPECIFIC INFORMATION.  
2. HEAVY DUTY PAVEMENT TO BE INSTALLED AT MAIN DRIVE AISLE (INCLUDING WITHIN SCDDT RIGHT-OF-WAY) AS SHOWN ON LAYOUT PLAN.  
3. ASPHALT CONCRETE AND ALL CRUSHED STONE MATERIALS SHOULD CONFORM TO THE SCDOT STANDARD SPECIFICATIONS.

ASPHALT PAVEMENT SECTION DETAIL (HEAVY DUTY)  
NOT TO SCALE

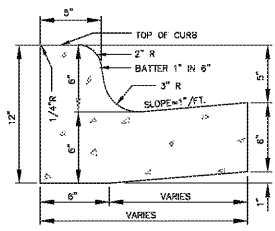


NOTES:  
1. PAVEMENT DESIGN PER GEOTECHNICAL REPORT PREPARED BY TERRACON DATED OCTOBER 10, 2016. REFER TO REPORT FOR SPECIFIC INFORMATION.  
2. HEAVY DUTY PAVEMENT TO BE INSTALLED AT MAIN DRIVE AISLE (INCLUDING WITHIN SCDDT RIGHT-OF-WAY) AS SHOWN ON LAYOUT PLAN.  
3. ASPHALT CONCRETE AND ALL CRUSHED STONE MATERIALS SHOULD CONFORM TO THE SCDOT STANDARD SPECIFICATIONS.

ASPHALT PAVEMENT SECTION DETAIL (WITHIN SCDDT R/W)  
NOT TO SCALE



CURB TAPER DETAIL  
NOT TO SCALE



NORMAL GUTTER

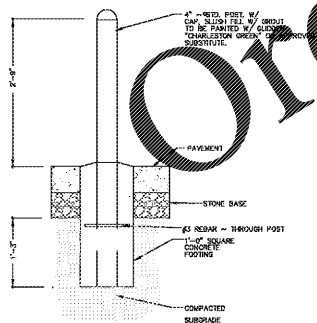
PITCHED GUTTER

NOTES:

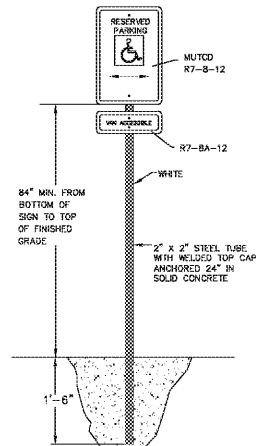
- 1. CURB AND GUTTER TO BE CONSTRUCTED IN 10'-FOOT LENGTHS.
- 2. 1/2 INCH EXPANSION JOINT SHALL BE PROVIDED AT INTERVALS NOT TO EXCEED 100 FEET, AT THE ENDS AND MID-POINT OF RETURNS, AND AT ANY POINT WHERE THE NEW CURB AND GUTTER ABUTS OTHER CONCRETE STRUCTURES.
- 3. 5'-FOOT LONG TRANSITIONS SHALL BE PROVIDED BETWEEN NORMAL GUTTER AND PITCHED GUTTER, UNLESS OTHERWISE NOTED ON THE PAVING PLANS.

BARRIER TYPE CURB AND GUTTER DETAIL

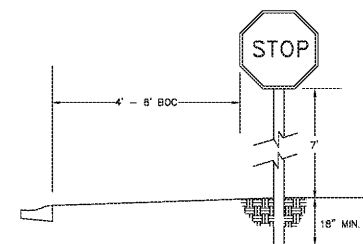
NOT TO SCALE



BOLLARD DETAIL  
NOT TO SCALE



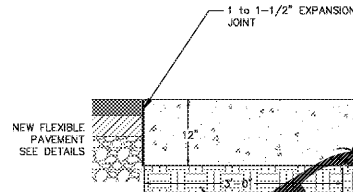
ADA SIGN DETAIL  
NOT TO SCALE



STOP SIGN DETAIL  
NOT TO SCALE

NOTES:

- 1. SIGN TO HAVE A RED REFLECTORIZED BACKGROUND WITH A WHITE REFLECTORIZED LEGEND AND BORDER.
- 2. USE M.U.T.C.D. TYPE RT-1-36 36"x36" SIGN MOUNTED ON GALVANIZED U-CHANNEL POSTS.
- 3. THE SIGN SHALL BE ATTACHED WITH A CAST ALUMINUM BRACKET WHICH FITS OVER THE END OF THE POST AND IS HELD IN PLACE BY ALLEN HEAD SET SCREWS. USE VULCAN SIGNS "VS-LOCK CROSS" OR APPROVED EQUIVALENT.
- 4. ALL SIGNS SHALL BE PLACED BETWEEN 4 AND 8 FEET FROM THE BACK OF CURB WITH HEIGHT OF SIGN TO BE 7'.
- 5. SIGN SHALL COMPLY WITH THE SOUTH CAROLINA MANUAL ON VISION, TRAFFIC CONTROL DEVICES.

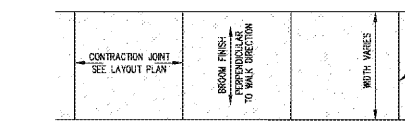


NEW FLEXIBLE PAVEMENT  
SEE DETAILS

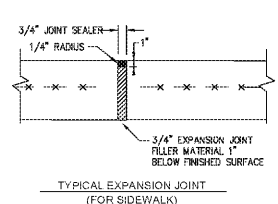
RIGID PAVEMENT AT FLEXIBLE SECTION

NOT TO SCALE

NOTES:  
1. PAVEMENT DESIGN PER GEOTECHNICAL REPORT PREPARED BY EGS DATED DECEMBER 14, 2016. REFER TO REPORT FOR SPECIFIC INFORMATION.  
2. HEAVY DUTY PAVEMENT TO BE INSTALLED AT MAIN DRIVE AISLE FROM ATLAS COURT TO SOUTH OF CONCRETE PAVEMENT SECTION.  
3. ASPHALT CONCRETE AND ALL CRUSHED STONE MATERIALS SHOULD CONFORM TO THE SCDOT STANDARD SPECIFICATIONS.



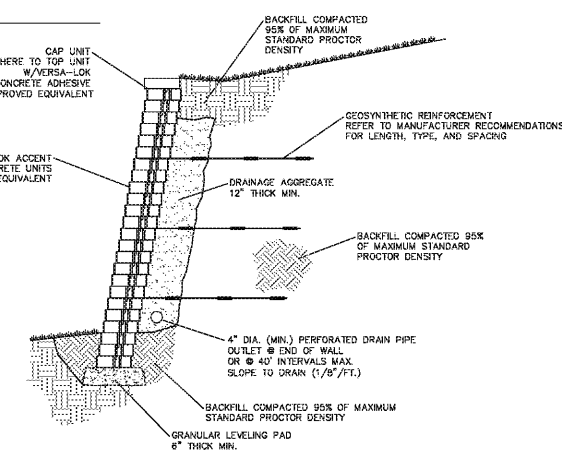
NOTE:  
EXPANSION JOINTS SHALL BE PROVIDED EVERY 30' IN ACCORDANCE WITH TYPICAL EXPANSION JOINT DETAIL.



NOTES:

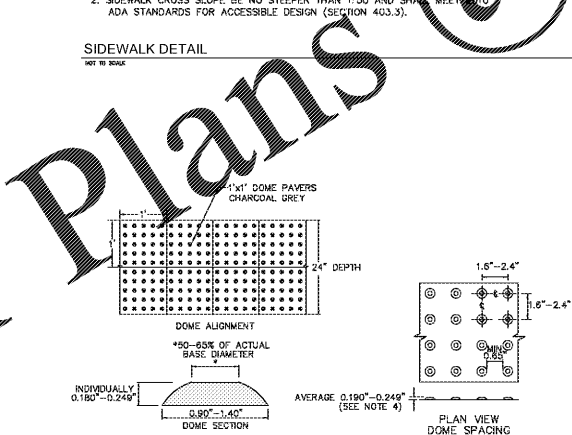
- 1. CONCRETE SCORING 5' O.C. AND EXPANSION JOINTS AT 30' O.C.
- 2. SIDEWALK CROSS SLOPE BE NO STEEPER THAN 1:50 AND SHALL MEET ADA STANDARDS FOR ACCESSIBLE DESIGN (SECTION 403.3).

SIDEWALK DETAIL  
NOT TO SCALE



TYPICAL SEGMENTAL BLOCK RETAINING WALL DETAIL  
NOT TO SCALE

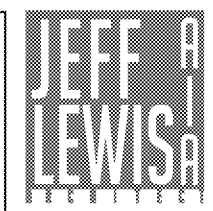
NOTES:  
1. THIS DETAIL IS CONCEPTUAL. CONTRACTOR TO PROVIDE RETAINING WALL DESIGN WITH AN ENGINEERED STAMPED DRAWING PROVIDED DURING SHOP DRAWING REVIEW.  
2. WEEP HOLES TO BE SPACED ALONG RETAINING WALL EVERY 40' OR AS RECOMMENDED BY MANUFACTURER.  
3. CONTRACTOR TO ENSURE DRAINAGE AWAY FROM WALL AND PREVENT SURFACE PONDING (TOP AND BOTTOM OF WALL).  
4. CONTRACTOR TO OUTFALL SWALE LOCATED BEHIND TOP OF WALL AROUND ENDS OF WALL SUCH TO PREVENT UNDERMINING FOUNDATION OF WALL.  
5. RETAINING WALL TO BE DESIGNED BY OTHERS (I.E. SEGMENTAL WALL MANUFACTURER). THIS DETAIL IS FOR REFERENCE ONLY.  
6. REFER TO GEOTECHNICAL REPORT PREPARED BY TERRACON DATED OCTOBER 10, 2016 FOR SPECIFIC INFORMATION REGARDING EXISTING SOIL CHARACTERISTICS.



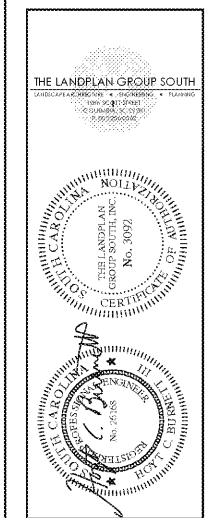
DETECTABLE WARNING SURFACE DETAIL NOTES

- 1. DETECTABLE WARNING SURFACES SHALL BE PROVIDED AT THE FOLLOWING LOCATIONS:  
(A) WHERE A SIDEWALK CROSSES A VEHICULAR WAY, EXCLUDING UN-SIGNALIZED DRIVEWAY CROSSINGS.  
(B) WHERE A RAIL SYSTEM CROSSES A PEDESTRIAN FACILITY THAT IS NOT SHARED WITH VEHICLES.  
(C) AT ISLANDS AND MEDIANS THAT ARE CUT THROUGH LEVEL WITH THE ROADWAY.
- 2. AT CURB RAMPS AND BLENDED TRANSITIONS, DETECTABLE WARNING SURFACES SHALL BE LOCATED SO THE EDGE NEAREST THE CURB LINE IS 6" MINIMUM AND 6" MAXIMUM DISTANCE FROM THE CURB LINE. THE SURFACE SHALL EXTEND A MINIMUM OF 24" ONTO THE RAMP IN THE DIRECTION OF TRAVEL FOR THE FULL WIDTH OF THE CURB RAMP.
- 3. AT RAIL CROSSINGS, THE DETECTABLE WARNING SURFACE SHALL BE 24" DEEP AND FULL WIDTH OF THE SIDEWALK. THE EDGE OF THE SURFACE NEAREST THE RAIL CROSSING IS 6" MINIMUM AND 6" MAXIMUM DISTANCE FROM THE VEHICLE DYNAMIC ENVELOPE (6' FROM THE NEAREST RAIL), UNLESS OTHERWISE NOTED BY THE RAILROAD.
- 4. DETECTABLE WARNING SURFACES SHALL CONSIST OF AN AREA OF TRUNCATED DOMES ALIGNED IN A SQUARE GRID PATTERN. THE BASE OF THESE DOMES SHALL BE 0.80"-1.40". THE TOP DIAMETER OF DOMES SHALL BE 30%-100% OF ACTUAL BASE DIAMETER, WITH A MINIMUM INDIVIDUAL HEIGHT OF 0.180" AND A MAXIMUM HEIGHT OF 0.249". THE AVERAGE OF TEN (10) INDIVIDUAL NODES PICKED AT RANDOM IN ANY SQUARE FOOT AREA SHALL BE 0.190"-0.249". THE DETECTABLE WARNING DOMES SHALL HAVE A CENTER-TO-CENTER SPACING OF 1.6"-2.4".
- 5. WHEN DOMES IN ANY ONE FOOT SQUARE AREA WEAR TO AN AVERAGE HEIGHT BELOW 0.101", THE SURFACE IS TO BE REPLACED.
- 6. FOR ALL DETECTABLE WARNING SURFACES, THE DEPARTMENT'S DEFAULT COLOR IS SAFETY YELLOW (FEDERAL NUMBER 33538) UNLESS A SPECIFIC PROJECT REQUIRES ANOTHER COLOR. WHEN A COLOR OTHER THAN SAFETY YELLOW IS SELECTED FOR A PROJECT, THE COLOR SELECTED WILL CONTRAST EITHER LIGHT-ON-DARK OR DARK-ON-LIGHT.
- 7. DETECTABLE WARNING SURFACE TO BE SET IN CONCRETE TO BE FLUSH WITH SURROUNDING GRADE.

DETECTABLE WARNING SURFACE DETAIL  
NOT TO SCALE



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LUGOFF FACILITY EXPANSION  
SANDHILLS MEDICAL FOUNDATION  
28 BALDWIN AVENUE  
LUGOFF, SOUTH CAROLINA 29078

REV.	DATE	DESCRIPTION
1.	11.22.19	Revised based on changes to FEMA flood maps.
2.	8.11.20	Bid Set

JOB # 16-039-1 07.15.2019  
ULPGS NO. 803-A

DETAILS  
C8.0