

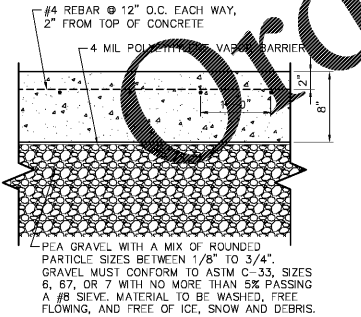
LEGEND:

- ⊙ = PART NUMBER; SEE SHEET F-1
- # = KEYNOTE; SEE SHEET F-1
- ⊠ = CONDUIT; SEE SHEET FE-1
- ⊘ = DETAIL CALLOUT
- ⊕ = ELEVATION CALLOUT
- ⊗ = SECTION CALLOUT

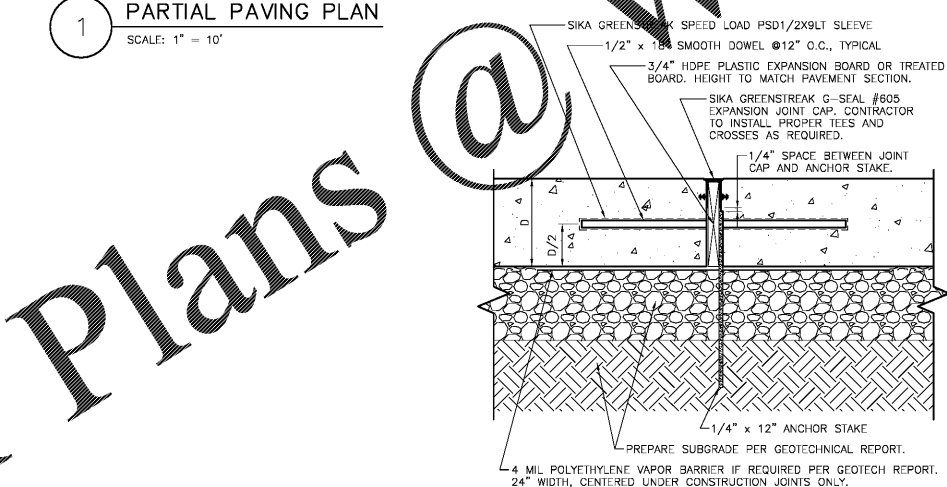
GENERAL PAVING NOTES

1. PREPARE SUBGRADE PER GEOTECH REPORT.
2. THIS PLAN IS FOR RELATIONSHIP LAYOUT AND SCOPE OF WORK PURPOSES ONLY. REFER TO SITE PLAN FOR RELATIONSHIP OF THESE STRUCTURES TO EXISTING CONDITIONS AND REFERENCE POINTS.
3. 1/2" GALV. EXPANSION JOINT MATERIAL SHALL SURROUND THE CANOPY COLUMN, BUMPERS, BOLLARDS AND DISPENSER ISLANDS AT GRADE.
4. DOWELED JOINTS SHALL BE LOCATED ON 12" CENTERS, ALONG THE INSIDE OF THE MOST EXTERIOR SLABS. DOWELS SHALL BE SMOOTH TYPE AND INSERTED INTO GREENSTREAK SPEED DOWEL FORMS. THE DOWEL IS LOCATED AT MID-SLAB DEPTH AND HELD IN PLACE LEVEL AND PERPENDICULAR TO THE JOINT USING SPEED DOWEL, WHICH REMAINS WITH THE FORM. AFTER THE FORMS ARE STRIPPED, THE DOWELS SHOULD BE CHECKED TO ASSURE FULL INSERTION AND ALIGNMENT IN THE INITIAL SLAB BEFORE POURING THE ADJACENT SLAB. DOWELS OCCURRING UNDER AND PARALLEL TO CONTROL JOINTS SHOULD BE DELETED.
5. TANK SLAB SHALL EXTEND 2'-0" (MIN.) BEYOND TANK EDGE IN ALL DIRECTIONS.
6. CHAMFER CORNER WHEN MEETING ASPHALT PAVING ONLY (24" EACH SIDE). SQUARE CORNERS WHERE MEETING CONCRETE PAVING.
7. REINFORCE CONCRETE SLAB AROUND MANHOLE WITH 4 - #5 REBAR 96" IN LENGTH ON EACH OF FOUR SIDES. PLACE REBAR 6" FROM SIDES OF SKIRT BOX. REINFORCING BARS TO BE A MINIMUM OF 2" AND MAX OF 4" FROM TOP OF CONCRETE. BARS SHALL BE SET ON A 45° ANGLE TO TYPICAL REINFORCING SHOWN IN DETAIL 2.

1 PARTIAL PAVING PLAN
SCALE: 1" = 10'

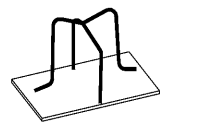


2 TANK PIT SLAB SECTION
SCALE: NOT TO SCALE

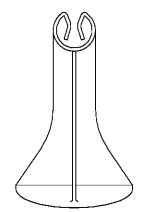


- INSTALLATION STEPS:**
1. TERMINATE ANCHOR STAKES BELOW THE TOP OF THE BOARD A DISTANCE EQUAL TO THE DEPTH OF THE G-SEAL PLUS 1/4" MIN.
WARNING: ANCHOR STAKES LONGER THAN 12" SHALL NOT BE USED AND CARE SHOULD BE TAKEN NOT TO UNNECESSARILY CONFLICT WITH PRODUCT PIPING TRENCHES.
 2. USE FACTORY FABRICATIONS FOR INTERSECTIONS AND CHANGES OF DIRECTION. ENSURE ALL SECTIONS TO BE SPICED ARE CUT SQUARE AND BOND TOGETHER USING G-SEAL ADHESIVE.
 3. SECURE G-SEAL TO THE EXPANSION BOARD WITH STAPLES, NAILS, SCREWS BELOW THE PROFILE RIBS (APPROXIMATELY 18" ON CENTER) OR ADHESIVE TO PREVENT DISLOCATION DURING CONCRETE PLACEMENT.
 4. INSERT SPEED LOAD SLEEVES INTO HOLES DRILLED THROUGH EXPANSION BOARDS AT MINIMUM 2" BELOW TOP OF SLAB. ENSURE SPEED LOAD SLEEVES ARE SECURED IN EXPANSION BOARD WITH BASE FLANGE FLAT TO EXPANSION BOARD FACE.
 5. INSERT SMOOTH DOWEL INTO SPEED LOAD SLEEVES AND PLACE CONCRETE USING NORMAL PLACEMENT TECHNIQUES. UTILIZING THE G-SEAL AS A SCREED RAIL, VIBRATE CONCRETE TO ENSURE GOOD CONSOLIDATION AROUND G-SEAL, SPEED LOAD AND REBAR.

4 DOWEL CONSTRUCTION JOINT IN CONCRETE SLAB DETAIL
SCALE: NOT TO SCALE



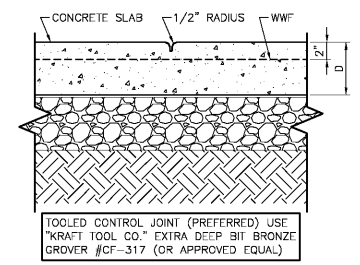
CHAIR SUPPORT (METAL WITH PLATE)



OPTIONAL CHAIR SUPPORT (PLASTIC)

1. CONTRACTOR SHALL UTILIZE THE APPROPRIATE SUPPORT DEVICE TO ACCEPT MESH OR REBAR REINFORCEMENT TYPES.
2. CHAIR SUPPORTS SHALL BE PLACED AT THREE FEET (3') O.C.E.W. MINIMUM SPACING.
3. SUPPORT SHALL BE SIZED TO PROVIDE ADEQUATE STRUCTURAL SUPPORT DURING CONSTRUCTION AND MAINTAIN THE REINFORCEMENT WITHIN THE CENTER OF THE PAVEMENT SECTION.

5 CHAIR SUPPORT FOR REINFORCED CONCRETE PAVEMENT
SCALE: NOT TO SCALE



6 TOOLED CONTROL JOINT
SCALE: NOT TO SCALE

Order Plans @

REFER TO CIVIL PLANS



PARTIAL PAVING PLAN	
DATE: JUNE, 2020	FILE NAME: 12-McG800/051-F-8
SCALE: AS NOTED	DATE:
No. REVISION	
NWC OAK DRIVE & HIGHWAY 1 LEXINGTON SOUTH CAROLINA	



COPYRIGHT
THESE PLANS ARE AN INSTRUMENT OF SERVICE AND ARE THE PROPERTY OF GALLOWAY, AND MAY NOT BE DUPLICATED, DISCLOSED, OR REPRODUCED WITHOUT THE WRITTEN CONSENT OF GALLOWAY. COPYRIGHTS AND INSTRUMENTS WILL BE ENFORCED AND PROSECUTED.

MURPHY EXPRESS
MURPHY OIL U.S.A. INC.
200 EAST PEACH STREET
EL DORADO, ARKANSAS 71730

H:\Murphy Oil Tank & Piping\SC Lexington EXP (8983) (HW 1) - Mc800751\Final\12-Mc800751_F-8.dwg - Dylar Petersen - 6/4/2020 4:29 PM

1/8" = 1" scale