

EROSION CONTROL STATEMENTS

"THE ESCAPE OF SEDIMENT FROM THE SITE SHALL BE PREVENTED BY THE INSTALLATION OF EROSION AND SEDIMENT CONTROL MEASURES AND PRACTICES PRIOR TO LAND DISTURBING ACTIVITIES."

"EROSION CONTROL MEASURES WILL BE MAINTAINED AT ALL TIMES. IF FULL IMPLEMENTATION OF THE APPROVED PLAN DOES NOT PROVIDE FOR EFFECTIVE EROSION CONTROL, ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO CONTROL OR TREAT THE SEDIMENT SOURCE."

"ANY DISTURBED AREA LEFT EXPOSED FOR A PERIOD GREATER THAN 14 DAYS SHALL BE STABILIZED WITH MULCH OR TEMPORARY SEEDING."

"TEMPORARY EROSION CONTROL MATTING REQUIRED ON ALL 2:1 SLOPES. PERMANENT EROSION CONTROL MATTING REQUIRED IN ALL DITCHES."

CONTACT INFORMATION

PRIMARY PERMITTEE
MARIO EVANS, AIRPORT DIRECTOR
2000 AIRPORT ROAD
ATLANTA, GA 30341
770-936-5440

24 HOUR CONTACT
TBD
TBD

EROSION CONTROL NOTES

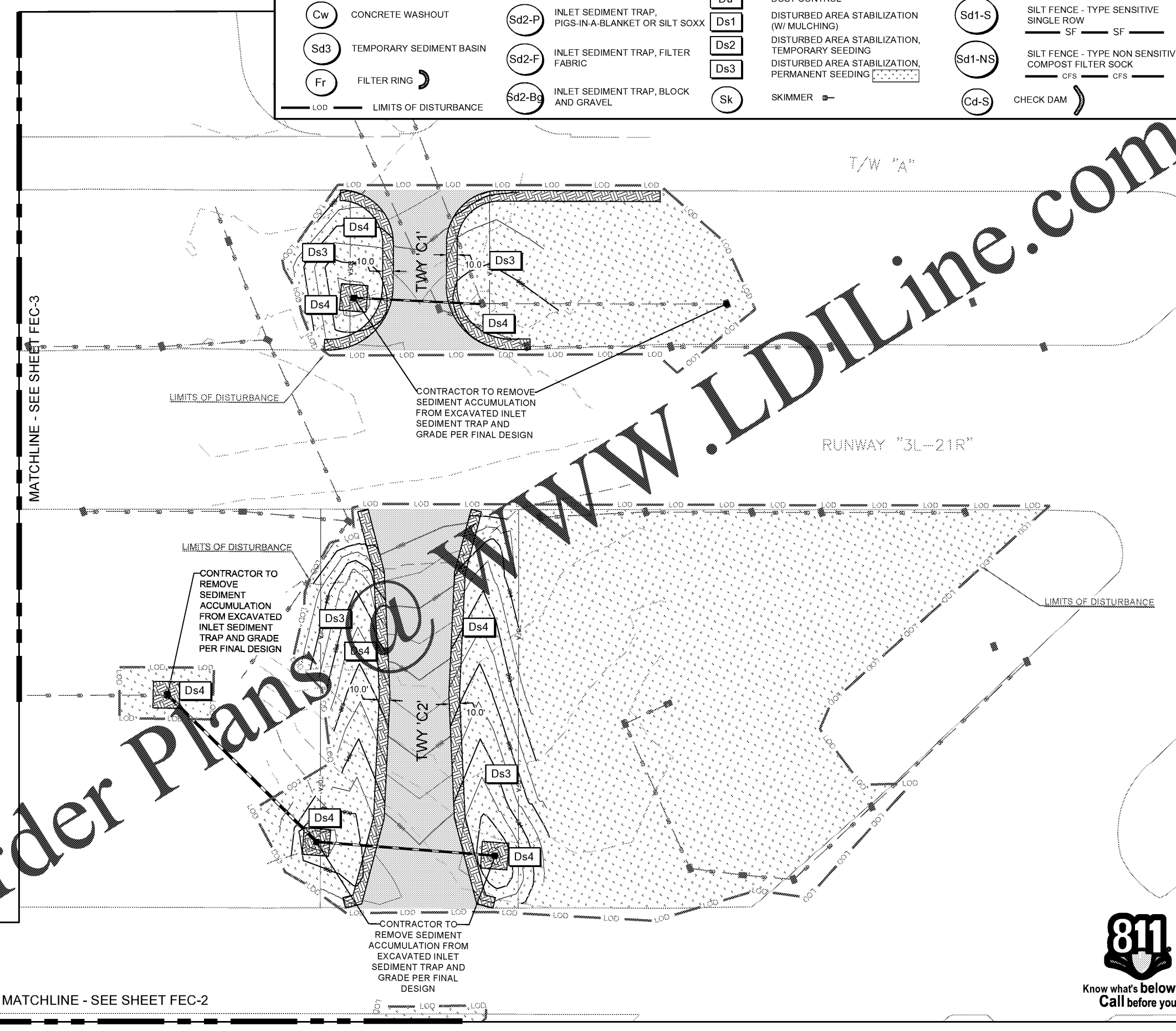
- LIMITS OF DISTURBANCE - FINAL PHASE: 11.8 ACRES
- CONTRACTOR SHALL MAINTAIN EXISTING EROSION CONTROL BMP'S. ALL EXISTING BMP'S SHALL BE CONSIDERED INCIDENTAL TO MOBILIZATION
- ALL EROSION CONTROL MEASURES SHOWN FOR INSTALLATION IN THE TSA AND RSA SHALL BE INSTALLED AFTER ASSOCIATED RUNWAY OR TAXIWAY CLOSURE AND SHALL BE REMOVED PRIOR TO REOPENING OF THE ASSOCIATED FACILITY.

FINAL PHASE SEQUENCE

- MAINTAIN ALL BMP'S FROM PREVIOUS PHASES.
- NOTIFY DESIGN ENGINEER IF ANY SEDIMENT EXITS THE PROJECT LIMITS.
- PRIOR TO REMOVING ANY BMP'S, MAINTAIN BMP'S AND REMOVE SEDIMENT TO HAUL OFFSITE SO THAT WHEN BMP'S ARE REMOVED, NO ADDITIONAL SEDIMENT WILL ESCAPE DOWNSTREAM.
- REMOVE ALL BMP'S THAT ARE IN AREAS THAT HAVE UNDERGONE FINAL STABILIZATION. KEEP BMP'S IN PLACE WHERE UPSTREAM AREAS DO NOT HAVE 70% COVERAGE IN 100% OF THE AREA. REMOVE THESE BMP'S ONLY AFTER THE AREAS HAVE REACHED FINAL STABILIZATION.
- MAINTAIN AND CLEAN UP ALL EROSION CONTROL STRUCTURES UNTIL FINAL STABILIZATION. INSTALL ADDITIONAL MEASURES AS NEEDED.
- STABILIZE AREAS WITH PERMANENT VEGETATION (DS3) AND SODDING (DS4).
- ONCE SITE HAS REACHED FINAL STABILIZATION, NOTIFY ENGINEER AND AIRPORT FOR INSPECTION PRIOR TO FILING THE NOT.
- CONSTRUCTION SEQUENCE IS RECOMMENDED BUT DOES NOT RELIEVE THE CONTRACTOR OF

EROSION CONTROL LEGEND

(Co) CONSTRUCTION ENTRANCE	(X) PAVEMENT TO BE REMOVED	(Du) DUST CONTROL	(Ds4) DISTURBED AREA STABILIZATION, SODDING
(Cw) CONCRETE WASHOUT	(Sd2-P) INLET SEDIMENT TRAP, PIGS-IN-A-BLANKET OR SILT SOXX	(Ds1) DISTURBED AREA STABILIZATION (W/ MULCHING)	(Sd1-S) SILT FENCE - TYPE SENSITIVE SINGLE ROW
(Sd3) TEMPORARY SEDIMENT BASIN	(Sd2-F) INLET SEDIMENT TRAP, FILTER FABRIC	(Ds2) DISTURBED AREA STABILIZATION, TEMPORARY SEEDING	(Sd1-NS) SILT FENCE - TYPE NON SENSITIVE COMPOST FILTER SOCK
(Fr) FILTER RING	(Sd2-Bg) INLET SEDIMENT TRAP, BLOCK AND GRAVEL	(Ds3) DISTURBED AREA STABILIZATION, PERMANENT SEEDING	(CFS) CFS
(LOD) LIMITS OF DISTURBANCE	(Sk) SKIMMER	(Cd-S) CHECK DAM	



MATCHLINE - SEE SHEET FEC-2

MATCHLINE - SEE SHEET FEC-3



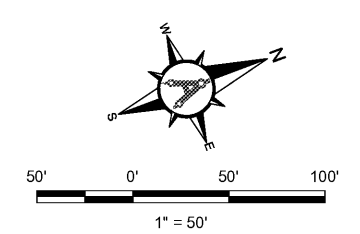
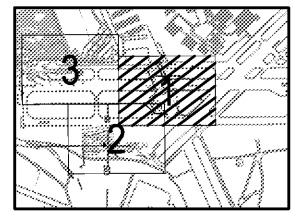
Michael Baker INTERNATIONAL

Designer: **G. SUMMERS**
Technician: **W. MCNAMARA**
Checked by: **D. SKURKY**
Project Number: **174297**



AULICK ENGINEERING LLC
HYDRAULICS & HYDROLOGY | EROSION CONTROL
AIRFIELD DESIGN | CONSTRUCTION MANAGEMENT

Notes:



REVISIONS			
No.	Description	Date	By

Project Name: **RUNWAY INCURSION MITIGATION IMPROVEMENTS (PDK 11)**

Drawing Name: **ES&PCP FINAL PHASE NO. 1**

ITB# 20-101257
Date: **FEBRUARY, 2020** Sheet Number: **13** of **24**
Scale: **1" = 50'** Drawing Number: **FEC-1**

