



**DEKALB PEACHTREE AIRPORT**  
DEKALB COUNTY, GEORGIA

**Michael Baker INTERNATIONAL**

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Project Number:

**174297**



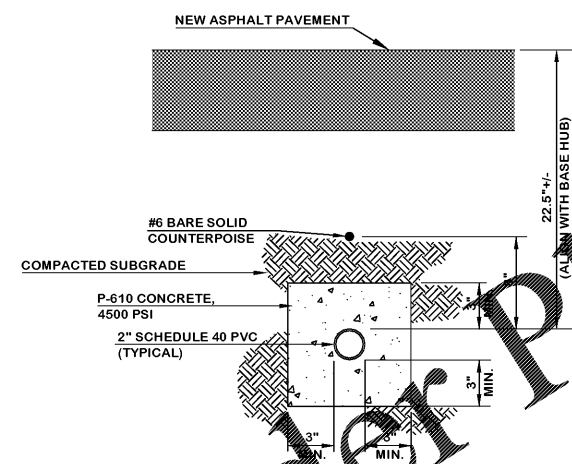
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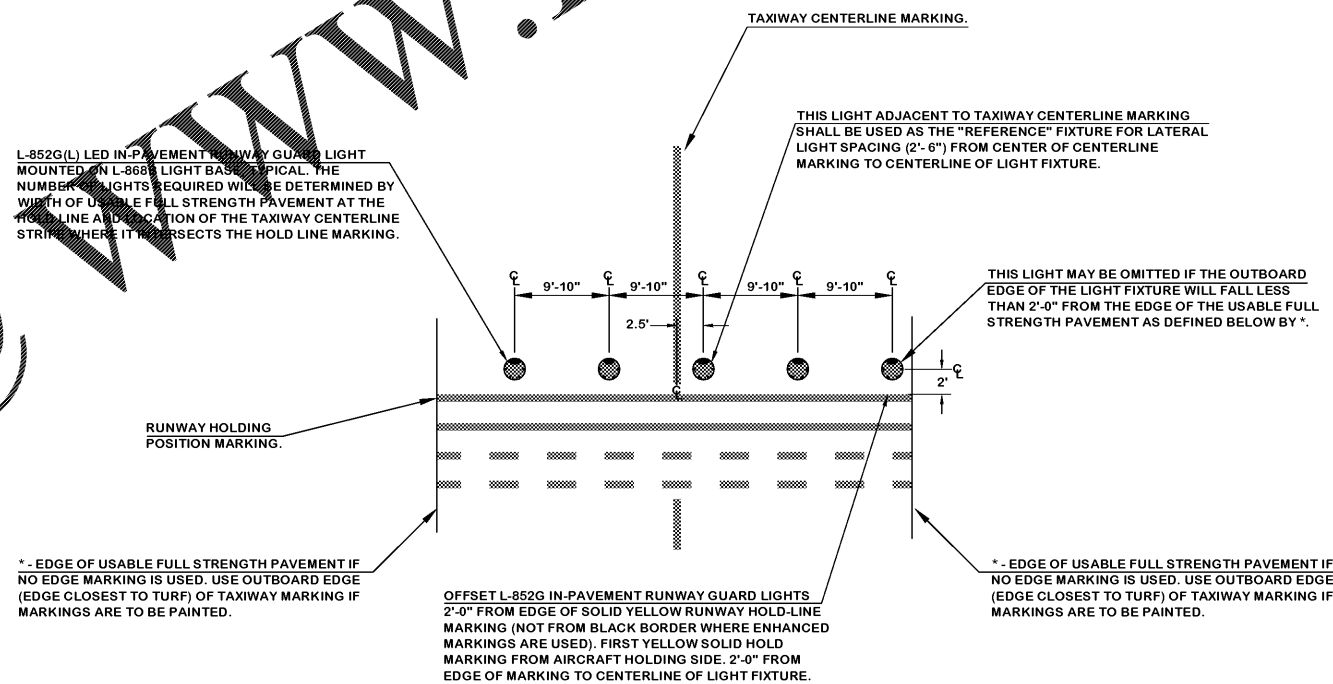
**L-852G IN-PAVEMENT RUNWAY GUARD LIGHT NOTES**

1. ALL LIGHT LOCATIONS ARE TO BE COORDINATED WITH HOLD BAR PAINT MARKINGS AND CENTERLINE PAINT MARKINGS. DO NOT INSTALL IN-PAVEMENT FIXTURES UNTIL EXACT PAINT MARKINGS ARE DETERMINED AND APPROVED BY THE AIRPORT'S AUTHORIZED REPRESENTATIVE. THE IN-PAVEMENT RUNWAY GUARD LIGHTING LAYOUT IS BASED ON THE "REFERENCE" FIXTURE WHICH IS DETERMINE FROM THE CENTERLINE PAINT MARKING.
2. "REFERENCE" FIXTURE SHALL BE 2'-6" FROM CENTER OF FIXTURE TO CENTER OF TAXIWAY CENTERLINE MARKING. THE CLOSEST EDGE OF THE "REFERENCE" FIXTURE TO THE TAXIWAY CENTERLINE MARKING CANNOT EXCEED 2'-0". THE "REFERENCE" FIXTURE SHALL BE ON THE SAME SIDE OF THE TAXIWAY CENTERLINE MARKING AS THE EXISTING TAXIWAY CENTERLINE FIXTURE WHERE POSSIBLE OR PLANNED TAXIWAY CENTERLINE FIXTURE.
3. THE OTHER FIXTURES ARE SPACED OFF OF THE "REFERENCE" FIXTURE. THESE FIXTURES SHALL BE INSTALLED EQUALLY AT 9'-10" CENTER TO CENTER. A +/-2" TOLERANCE IS ALLOWED. IF A FIXTURE FALLS AT A POINT LESS THEN 2 FEET FROM THE DEFINED EDGE OF PAVEMENT, IT SHALL BE OMITTED.
4. FIXTURES SHALL BE INSTALLED PARALLEL TO THE HOLD BAR MARKING. THE FIXTURES SHALL BE 2'-0" FROM THE CENTER OF THE FIXTURES TO THE CLOSEST EDGE OF THE FIRST SOLID PAINT MARKING ON THE HOLD SIDE. A TOLERANCE OF +/-2" IS ALLOWED.
5. FIXTURE BASES SHALL BE LOCATED NO CLOSER THAN 2'-0" FROM THE EDGE OF THE BASE TO A RIGID PAVEMENT JOINT. IF A CONFLICT OCCURS, THE PAINT MARKINGS AND LIGHT FIXTURES SHALL BE MOVED AWAY FROM THE RUNWAY AT THE MINIMUM DISTANCE REQUIRED TO RESOLVE THE CONFLICT.
6. THE LIGHT BEAM OF THE FIXTURES SHALL BE PERPENDICULAR TO THE RUNWAY HOLDING POSITION MARKINGS. A TOLERANCE OF +/-1 DEGREE IS ALLOWED. THE CONTRACTOR SHALL UTILIZE 12-BOLT LIGHT BASES. A LINE THROUGH ONE PAIR OF BOLTS HOLES ON THE OPPOSITE SIDE OF THE BASE SHALL BE PARALLEL TO THE RUNWAY HOLDING POSITION MARKINGS.

NOTE: IN-PAVEMENT RUNWAY GUARD LIGHT LOCATIONS ARE BASED ON THE PAINTED HOLD BAR LOCATIONS AND PAINTED TAXIWAY CENTERLINE LOCATIONS. ELECTRIC CONDUIT SHALL NOT BE Laid OUT OR MARK IN-PAVEMENT RUNWAY GUARD LIGHT LOCATIONS UNTIL THE HOLD BAR PAINT MARKINGS AND CENTERLINE PAINT MARKINGS ARE LAID OUT AND CONFIRMED BY THE PAINT CONTRACTOR AND APPROVED BY THE AIRPORT'S AUTHORIZED REPRESENTATIVE.



**CONDUIT WITH CONCRETE ENCASEMENT UNDER ACTIVE PAVEMENT**  
N.T.S.



**TYPICAL L-852G IN-PAVEMENT RUNWAY GUARD LIGHT LAYOUT DETAIL**  
N.T.S.

LIGHT INSTALLATION SHALL BE ORIENTED SO THE LIGHTS ARE VISIBLE FOR TRAFFIC ON THE TAXIWAY APPROACHING THE RUNWAY.

REVISIONS			
No.	Description	Date	By

Project Name:  
**RUNWAY INCURSION MITIGATION IMPROVEMENTS (PDK 11)**

Drawing Name:  
**LIGHTING DETAILS PLAN NO. 5**

ITB# 20-101257

Date: <b>FEBRUARY, 2020</b>	Sheet Number: <b>64</b> of <b>72</b>
Scale: <b>NTS</b>	Drawing Number: <b>LD-5</b>