

**ROLL BACK CURB**

**VERTICAL CURB**

NOTES:

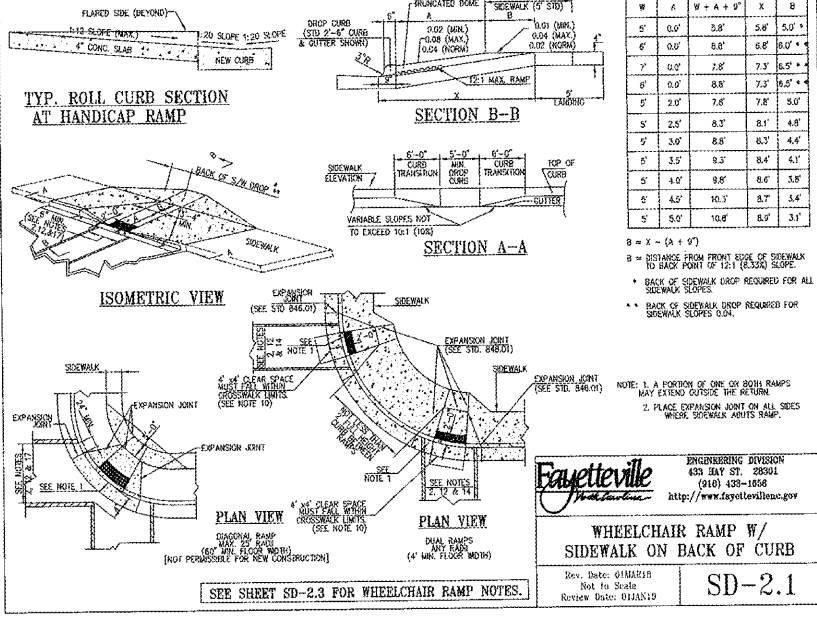
- VERTICAL CURB - GRASS UTILITY STRIP MAY BE REMOVED UPON APPROVAL OF THE CITY ENGINEER. IF NO GRASS UTILITY STRIP, INSTALL 1/2" EXPANSION JOINT MATERIAL BETWEEN BACK OF CURB AND SIDEWALK.
- SEE DETAIL SD-3 CONTRACTION AND EXPANSION JOINT.
- SEE DETAIL SD-9 TYPICAL RESIDENTIAL DRIVEWAY APPROX AND DETAIL SD-11 TYPICAL COMMERCIAL DRIVEWAY APPROX.
- SODDING LIMITS INCLUDE THE GRASS UTILITY STRIP BETWEEN THE BACK OF CURB AND THE SIDEWALK AS WELL AS 2'6" BEYOND THE SIDEWALK.
- SEE TECHNICAL SPECIFICATION SECTION 06455, PART 3, SUB-SECTION 3.02, SUB-PART F.

**Fayetteville** ENGINEERING DIVISION  
433 HAY ST. 28301  
(910) 433-1656  
http://www.fayettevillenc.gov

TYPICAL SIDEWALK SECTION & SODDING LIMITS

Rev. Date: 310CT18  
Not to Scale  
Review Date: 01JAN19

**SD-12**



**TYP. ROLL CURB SECTION AT HANDICAP RAMP**

**SECTION B-B**

**SECTION A-A**

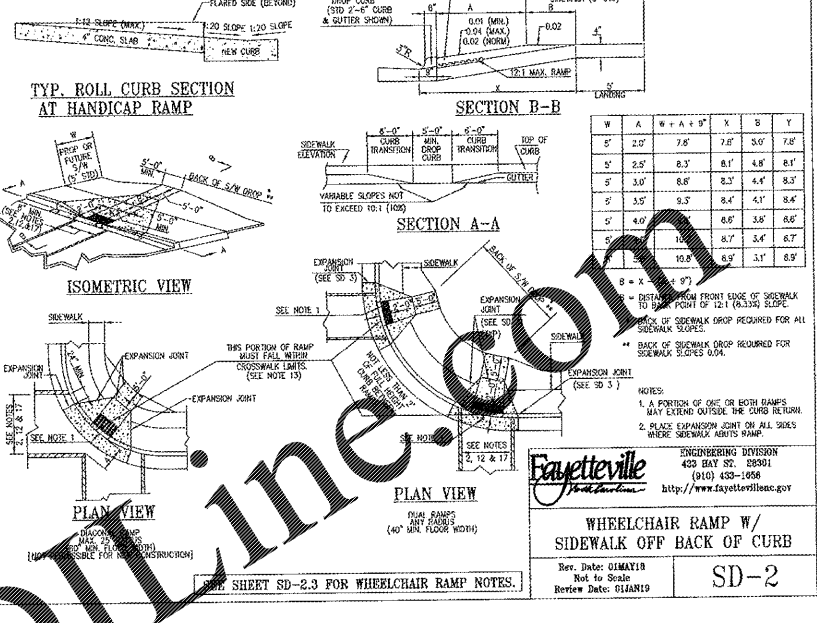
**ISOMETRIC VIEW**

**PLAN VIEW**

**WHEELCHAIR RAMP W/ SIDEWALK ON BACK OF CURB**

Rev. Date: 01MAY18  
Not to Scale  
Review Date: 01JAN19

**SD-2.1**



**TYP. ROLL CURB SECTION AT HANDICAP RAMP**

**SECTION B-B**

**SECTION A-A**

**ISOMETRIC VIEW**

**PLAN VIEW**

**WHEELCHAIR RAMP W/ SIDEWALK OFF BACK OF CURB**

Rev. Date: 01MAY18  
Not to Scale  
Review Date: 01JAN19

**SD-2**

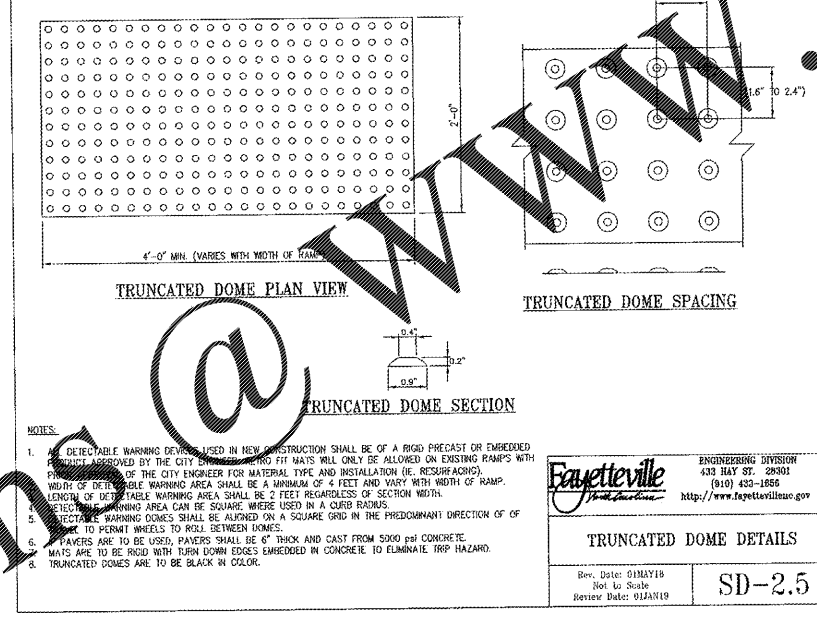
- CONSTRUCT THE WALKING SURFACE WITH SLIP RESISTANCE AND A 70% CONTRASTING COLOR TO THE SIDEWALK. WHEELCHAIR RAMP SHALL ALSO INCLUDE A BLACK, TRUNCATED DOME STRIP.
- CROSSWALK WIDTHS AND CONFIGURATION VARY BUT MUST CONFORM TO TRAFFIC DESIGN STANDARDS.
- NORTH CAROLINA GENERAL STATUTE 136-44.14 REQUIRES THAT ALL STREET CURBS BEING CONSTRUCTED OR RECONSTRUCTED FOR MAINTENANCE PROCEDURES, TRAFFIC OPERATIONS, REPAIRS, CORRECTION OF UTILITIES OR ALTERED FOR ANY REASON, AFTER SEPTEMBER 1, 1973 SHALL PROVIDE WHEELCHAIR RAMPS FOR THE PHYSICALLY DISABLED AT ALL INTERSECTIONS WHERE BOTH CURB AND CUTTER AND SIDEWALKS ARE PROVIDED AND AT OTHER POINTS OF PEDESTRIAN FLOW.
- IN ADDITION, SECTION 228 OF THE 1973 FEDERAL AID HIGHWAY SAFETY ACT REQUIRES PROVISION OF CURB RAMPS ON ANY CURB CONSTRUCTION AFTER JULY 1, 1976 WHETHER A SIDEWALK IS PROPOSED INITIALLY OR IS PLANNED FOR A FUTURE DATE.
- THE AMERICANS WITH DISABILITIES ACT (ADA) OF 1990 EXTENDS TO INDIVIDUALS WITH DISABILITIES. COMPREHENSIVE CIVIL RIGHTS PROTECTIONS SIMILAR TO THOSE PROVIDED TO PERSONS ON THE BASIS OF RACE, SEX, NATIONAL ORIGIN AND RELIGION UNDER THE CIVIL RIGHTS ACT OF 1964. THESE CURB RAMPS HAVE BEEN DESIGNED TO COMPLY WITH THE CURRENT ADA STANDARDS.
- PROVIDE WHEELCHAIR RAMPS AT LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. LOCATE WHEELCHAIR RAMPS AS DIRECTED BY THE ENGINEER WHERE EXISTING LIGHT POLES, FIRE HYDRANTS, STREET LIGHTS, ETC. AFFECT PLACEMENT. WHERE TWO RAMPS ARE INSTALLED NOT LESS THAN 2 FEET OF FULL HEIGHT CURB SHALL BE PLACED BETWEEN THE RAMPS. PLACE DUAL RAMPS AS NEAR PERPENDICULAR TO THE TRAVEL LANE BEING CROSSED AS POSSIBLE.
- PAY FOR ALL VARIABLE DEPTH CONCRETE USED FOR CONSTRUCTION OF WHEELCHAIR RAMPS AS CONCRETE WHEELCHAIR RAMPS AS DESCRIBED ON PROJECT SPECIFICATIONS.
- PAY FOR ALL DEPRESSED CURBS AT WHEELCHAIR RAMPS AS THE TYPE CURB AND CUTTER USED ADJACENT TO DEPRESSED CURB. (LN. FT.)
- SUCH PRICES AND PAYMENTS WILL BE CONSIDERED FULL COMPENSATION FOR ALL MATERIALS, LABOR, EQUIPMENT, TOOLS AND INCIDENTALS NECESSARY TO SATISFACTORILY COMPLETE THE WORK.
- DO NOT EXCEED 0.08 (1:12) SLOPE ON THE WHEELCHAIR RAMP IN RELATIONSHIP TO THE GRADE OF THE STREET.
- CONSTRUCT WHEELCHAIR RAMPS 48" (ADA STANDARD) OR GREATER FOR DUAL RAMPS.
- USE CLASS "B" CONCRETE WITH A SIDEWALK FINISH IN ORDER TO OBTAIN A ROUGH NON-SKID TYPE SURFACE.
- PLACE A 1/2" EXPANSION JOINT WHERE THE CONCRETE WHEELCHAIR RAMP JOINS THE CURB AND AS SHOWN ON SD-2.
- PLACE THE INSIDE PEDESTRIAN CROSSWALK LINES NO CLOSER IN THE INTERSECTION BY BISECTING THE INTERSECTION RADIUS, WITH ALLOWANCE OF A 4' CLEAR ZONE IN THE VEHICULAR TRAVELWAY WHEN ONE RAMP IS INSTALLED. (SEE NOTE 17)
- COORDINATE THE CURB CUT AND THE PEDESTRIAN CROSSWALK LINES SO THE FLOOR OF THE WHEELCHAIR RAMP WILL FALL WITHIN THE PEDESTRIAN CROSSWALK LINES. PLACE DIAGONAL RAMPS WITH FLARED SIDES SO 24" OF FULL HEIGHT CURB FALLS WITHIN THE CROSSWALK MARKINGS ON EACH SIDE OF THE FLARES.
- CONSTRUCT THE PEDESTRIAN CROSSWALK A MINIMUM OF 6 FEET. A CROSSWALK WIDTH OF 10 FEET OR GREATER IS DESIRABLE.
- USE STOP LINES, NORMALLY PERPENDICULAR TO THE LANE LINES, NOTING IT IS IMPORTANT TO INDICATE THE POINT BEHIND WHICH VEHICLES ARE REQUIRED TO STOP IN COMPLIANCE WITH A TRAFFIC SIGNAL, STOP SIGN OR OTHER LEGAL REQUIREMENT. AN UNUSUAL APPROACH SKEW MAY REQUIRE THE PLACEMENT OF THE STOP LINE TO BE PARALLEL TO THE INTERSECTING ROADWAY.
- TERMINATE PARKING A MINIMUM OF 20 FEET BACK OF PEDESTRIAN CROSSWALK.
- PLACE ALL PAYMENT MARKINGS IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION AND THE NORTH CAROLINA SUPPLEMENT TO THE MUTCD.

**Fayetteville** ENGINEERING DIVISION  
433 HAY ST. 28301  
(910) 433-1656  
http://www.fayettevillenc.gov

**WHEELCHAIR RAMP NOTES**

Rev. Date: 01JAN19  
Not to Scale  
Review Date: 01JAN19

**SD-2.3**



**TRUNCATED DOME PLAN VIEW**

**TRUNCATED DOME SPACING**

**TRUNCATED DOME SECTION**

NOTES:

- DETECTABLE WARNING DEVICES USED IN NEW CONSTRUCTION SHALL BE OF A RIGID PRECAST OR EMBEDED PRECAST ASSIGNED BY THE CITY ENGINEER. FIT MAIS WILL ONLY BE ALLOWED ON EXISTING RAMPS WITH PERMITS OF THE CITY ENGINEER FOR MATERIAL TYPE AND INSTALLATION (IE. RESURFACING).
- WIDTH OF DETECTABLE WARNING AREA SHALL BE A MINIMUM OF 4 FEET AND MUST MATCH WIDTH OF RAMP.
- LENGTH OF DETECTABLE WARNING AREA SHALL BE 2 FEET REGARDLESS OF SECTION WIDTH.
- DETECTABLE WARNING AREA CAN BE SQUARE WHERE USED IN A CURB RADIUS.
- RECTANGULAR WARNING DOMES SHALL BE ALIGNED ON A SQUARE GRID IN THE PREDOMINANT DIRECTION OF OF TRAFFIC TO PREVENT WHEELS TO ROLL BETWEEN DOMES.
- PAVERS ARE TO BE USED. PAVERS SHALL BE 6" THICK AND CAST FROM 5000 PSI CONCRETE.
- MAIS ARE TO BE RIGID WITH ROUNDED EDGES EMBEDDED IN CONCRETE TO ELIMINATE TRIP HAZARD.
- TRUNCATED DOMES ARE TO BE BLACK IN COLOR.

**Fayetteville** ENGINEERING DIVISION  
433 HAY ST. 28301  
(910) 433-1656  
http://www.fayettevillenc.gov

**TRUNCATED DOME DETAILS**

Rev. Date: 01MAY18  
Not to Scale  
Review Date: 01JAN19

**SD-2.5**

Order Plans @ [www.lidilinc.com](http://www.lidilinc.com)

**TRIANGLE SITE DESIGN**

CONSULTANT:  
TRIANGLE SITE DESIGN, PLLC  
4004 BARRETT DR, STE 101  
RALEIGH, NC 27609  
(919) 553-6570



*Matthew E. Lower*

OWNER/DEVELOPER:  
Revered Rd Fayetteville 2019, LLC  
Hartzog Holdings, LLC  
Attn: Collins Hartzog  
109 Still Wild Lane  
Elgin, SC 29045  
803-561-6718  
collins@hartzogholdings.com



**Starbucks**  
920 Strickland Bridge Road  
Fayetteville, NC  
Cumberland County

This document, together with the concepts and designs presented herein, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Triangle Site Design, PLLC shall be without liability to Triangle Site Design, PLLC.

REV.	DATE	DESCRIPTION
1	10/15/18	ISSUED FOR CITY OF FAYETTEVILLE REVIEW
2	10/29/18	ISSUED FOR SOIL PROFILES, TYPING AND LEGAL CONCERNS, PROGRESS COMMENTS
3	12/20/18	ISSUED PER FAYETTEVILLE PAV. REVIEW
4	1/17/19	ISSUED PER STARBUCKS REVIEW

Project No. 009016  
Date: November 1, 2019  
18a

**CONSTRUCTION DETAILS**

Sheet No. **C1.4B**