

**GENERAL NOTES**

- METRO PUBLIC WORKS STANDARDS SHALL BE UTILIZED FOR CONSTRUCTION IN THE RIGHT OF WAYS.
- THE CONTRACTOR SHALL PROVIDE ALL NECESSARY LABOR, MATERIAL, EQUIPMENT, TOOLS, AND SERVICES REQUIRED TO COMPLETE CONSTRUCTION AND MATERIAL TESTING FOR WORK WITHIN THE PUBLIC RIGHT-OF-WAY. ALL WORK SHALL BE PERFORMED IN A SAFE AND REASONABLE WORKING MANNER IN ACCORDANCE WITH BEST PRACTICES AND PROCEDURES.
- THE CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE LOCAL, STATE AND FEDERAL, ORDINANCES, REGULATIONS AND REQUIREMENTS NECESSARY TO COMPLETE THE WORK. THIS INCLUDES PROVISIONS FOR MAINTENANCE OF TRAFFIC, CONSTRUCTION, AND THE OCCUPATIONAL SAFETY AND HEALTH ACT (OSHA).
- ALL CONSTRUCTION METHODS, MATERIALS AND WORK ACTIVITIES SHALL BE IN ACCORDANCE WITH THE KENTUCKY TRANSPORTATION CABINET DEPARTMENT OF HIGHWAYS STANDARD SPECIFICATIONS AND SUPPLEMENT FOR ROAD AND BRIDGE CONSTRUCTION LATEST EDITION UNLESS OTHERWISE SPECIFIED. THE CURRENT KENTUCKY STANDARD DRAWINGS WILL ALSO APPLY UNLESS OTHERWISE NOTED ON THE PLANS.
- CONSTRUCTION BOND AND ENCROACHMENT PERMIT WILL BE REQUIRED BY METRO PUBLIC WORKS FOR ALL CONSTRUCTION WORK WITHIN ANY EXISTING RIGHTS-OF-WAY PRIOR TO CONSTRUCTION APPROVAL.
- FORTY EIGHT (48) HOURS PRIOR TO STARTING CONSTRUCTION A NOTICE SHALL BE GIVEN BY THE DEVELOPER OR CONTRACTOR TO METRO PUBLIC WORKS SUBDIVISION INSPECTOR FOR NECESSARY INSPECTION OF SUBGRADE, SUBBASE, BASE, PAVEMENT, SURFACING, ROADWAY AND CURB REPAIRS, AND SIDEWALKS WITHIN THE PUBLIC RIGHTS-OF-WAY. CALL THE INSPECTORS DIRECTLY OR THE MAIN PUBLIC WORKS TELEPHONE NUMBER 502-574-5810.
- AN ENCROACHMENT PERMIT MUST BE ISSUED BY THE LOUISVILLE METRO PUBLIC WORKS AND TRANSPORTATION DEPARTMENT FOR ALL UTILITY WORK AFFECTING THE LOUISVILLE METRO ROAD SYSTEM.
- EXISTING UTILITIES SHOWN ON THESE PLANS ARE APPROXIMATE. INDIVIDUAL SERVICE LINES ARE NOT SHOWN. THE CONTRACTOR OR SUBCONTRACTOR SHALL NOTIFY THE KENTUCKY UTILITY PROTECTION CENTER "KENTUCKY 811" (PHONE 811 OR TOLL FREE PHONE 1-800-752-6007) FORTY-EIGHT (48) HOURS IN ADVANCE OF ANY CONSTRUCTION ON THIS PROJECT. THIS NUMBER WAS ESTABLISHED TO PROVIDE ACCURATE LOCATIONS OF EXISTING BELOW GROUND UTILITIES (I.E. CABLES, ELECTRIC WIRES, GAS AND WATER LINES). THE CONTRACTOR SHALL BE RESPONSIBLE FOR BECOMING FAMILIAR WITH ALL UTILITY REQUIREMENTS SET FORTH ON THE PLANS IN THE TECHNICAL SPECIFICATIONS AND SPECIAL PROVISIONS.
- THE DEVELOPER IS RESPONSIBLE FOR THE RELOCATION AND ADJUSTMENT OF ALL UTILITY INSTALLATIONS. SHOULD ANY EXISTING DRAINAGE STRUCTURES AND/OR UTILITIES WITHIN THE RIGHT OF WAY NEED TO BE RELOCATED, EXTENDED OR OTHERWISE ALTERED, IT WILL BE AT THE OWNERS' OR DEVELOPERS' EXPENSE.

**PAVEMENTS**

- WHEN MATCHING EXISTING BITUMINOUS PAVEMENTS A TWO (2) INCH DEPTH SAWCUT A MINIMUM OF TWELVE (12) INCHES FROM THE EDGE OR TO SOUND MATERIAL AND ROADWAY ROCK BASE AND THE ASPHALT MATERIAL PLACED TO FORM A WATERTIGHT JOINT AS DIRECTED BY METRO PUBLIC WORKS.
- ASPHALT CONCRETE SURFACE SHALL BE CLASS 2, COMPACTED DEPTH AS SHOWN.
- ASPHALT CONCRETE BINDER SHALL BE CLASS 2, COMPACTED DEPTH AS SHOWN.
- STONE BASE AND SUBBASE SHALL BE PLACED AND COMPACTED IN SEPARATE COURSES.
- AN APPROVED JOINT SEALER IS TO BE USED TO SEAL ALL JOINTS BETWEEN THE NEW PAVEMENT AND EXISTING PAVEMENT IN ACCORDANCE WITH KENTUCKY DEPARTMENT OF HIGHWAYS STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.
- TACK COAT SHALL BE APPLIED AT THE RATE OF 0.1 GALLONS PER SQ. YD. WITH INSTALLATION OF SURFACE ASPHALT. WHEN THE FINAL SURFACE ASPHALT INSTALLATION OCCURS AT A DIFFERENT TIME THAN THAT OF THE ASPHALT BASE COURSE, AN ASPHALTIC TACK COAT SHALL BE APPLIED AT THE SAME RATE OF 0.1 GALLONS PER SQ. YD. THE ASPHALTIC TACK COAT SHALL BE APPLIED UNIFORMLY AND OUT TO THE EDGES OF THE BASE COURSE TO INSURE PROPER ADHESION OF SURFACES.

**SIDEWALKS AND CURBS**

- ALL CONCRETE TO BE A MINIMUM OF 3,500 PSI UNLESS OTHERWISE SPECIFIED.
- THE CONTRACTOR SHALL PROVIDE AN EXPANSION JOINT BETWEEN THE CURB AND GRATE ON ALL CURB INLETS.
- ALL SIDEWALKS TO BE CONSTRUCTED OF CLASS A CONCRETE AS SHOWN ON THE PLAN, FOUR (4) INCH THICKNESS AND SIX (6) INCHES THICKNESS AT ALL DRIVEWAYS AND ALLEYS.
- SIDEWALK CROSS SLOPE SHALL BE NO GREATER THAN 2% AND NO LESS THAN 1%.
- WHEN SIDEWALK ELEVATIONS DEVIATE SLIGHTLY FROM THE PROPOSED CONSTRUCTION PLANS ELEVATIONS, IT WILL BE THE RESPONSIBILITY OF THE DEVELOPER TO PAY FOR THE ADJUSTMENT OF THE UTILITY COVER, PAD OR GRATE WHICH WAS PREVIOUSLY SET TO THE CONSTRUCTION PLAN ELEVATIONS FOR SIDEWALKS, OR TO REMOVE THE SIDEWALK AND RECONSTRUCT AT PROPER ELEVATION.
- ALL CURBS ARE TO BE STRAIGHT LINES, SMOOTH CURVES OR UNIFORM RADII AS INDICATED ON THE PLANS. REFER TO THE KENTUCKY TRANSPORTATION CABINET DEPARTMENT OF HIGHWAYS STANDARD SPECIFICATIONS AND SUPPLEMENT FOR ROAD AND BRIDGE CONSTRUCTION LATEST EDITION FOR CONSTRUCTION TOLERANCES AND DEVIATIONS FROM THE PLANS.
- CONSTRUCT 1/2" EXPANSION JOINTS AT ALL BREAKS IN ALIGNMENT, AT ALL DRAINAGE BOXES AND OTHER FIXED OBJECTS, AT THE BEGINNING AND ENDING POINTS OF CURVES AND AT THE BEGINNING, QUARTER, MIDDLE AND ENDING POINTS OF SEMICIRCULAR CURVES.
- ON LONG STRAIGHT, LINEAR RUNS OF CURBING, CONSTRUCT EXPANSION JOINTS A MAXIMUM OF EVERY 30' ON CENTER AND CONTROL/SCORE JOINTS EVERY 10' ON CENTER.
- EXPANSION JOINTS WILL NOT BE REQUIRED AT THE QUARTER POINTS FOR SEMI CIRCLES HAVING RADII OF 5' OR LESS.

**DRAINAGE**

- ALL STORM SEWER PIPE SHALL BE A MINIMUM OF 12" DIAMETER, WITH MATERIAL TYPE AND CONSTRUCTION IN ACCORDANCE WITH MSD SPECIFICATIONS.
- BACKFILL AROUND DRAINAGE STRUCTURES AND TRENCH BACKFILL BELOW THE MIDPOINT OF CONCRETE PIPE CULVERTS SHALL BE #57 STONE. BACKFILL ABOVE THE MIDPOINT OF CONCRETE PIPE SHALL BE OF APPROVED MATERIALS AND COMPACTED TO EMBANKMENT REQUIREMENTS.
- THE CONTRACTOR MAY USE PRECAST STORM DRAINAGE STRUCTURES IF PROVIDED FROM SUPPLIER APPROVED BY MSD.
- PRIOR TO BEGINNING CONSTRUCTION, CONTRACTOR TO VERIFY LOCATION AND DEPTH OF EXISTING STORM STRUCTURES AND PIPES TO BE CONNECTED TO, AND NOTIFY ENGINEER IF DIFFERENT THAN INDICATED IN THE PLANS.

**SIGNAGE AND PAVEMENT MARKINGS**

- ALL SIGNS AND INSTALLATION SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, CURRENT EDITION.
- ALL SIGNS SHALL BE RETRO REFLECTIVE INCLUDING MESSAGE, BORDER, AND BACKGROUND.
- STREET SIGNS SHALL HAVE A WHITE LEGEND ON A GREEN BACKGROUND, UNLESS OTHERWISE APPROVED BY METRO PUBLIC WORKS.
- REFER TO SECTION 2A.14 LETTERING AND DIMENSIONS "STANDARD HIGHWAY SIGNS BOOK" "STANDARD ALPHABET FOR HIGHWAY SIGNS AND PAVEMENT MARKINGS"
- SIGN POSTS SHALL CONFORM TO KENTUCKY TRANSPORTATION CABINET/DEPARTMENT OF HIGHWAYS STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.
- ALL ARROWS, TRACKING AND STOP BARS SHALL BE THERMOPLASTIC, NOT PAINTED.
- CONTRACTOR SHALL INSTALL PAVEMENT MARKINGS FOR ENTIRE CONSTRUCTION LIMITS PER KDOT SPECIFICATIONS: KDOT NO. TSC-300-07 PAVEMENT MARKER ARRANGEMENTS MULTI-LANE ROADWAYS KDOT NO. TSC-400-03 PAVEMENT MARKER TYPE V
- ALL STRIPING AND MARKINGS ARE TO BE INSTALLED PER CURRENT KTC STANDARD SPECIFICATION, WHICH REQUIRE A PROVING PERIOD, AND MAY REQUIRE AN ADDITIONAL COATING AFTER 30-60 DAYS AFTER THE FINAL SURFACE IS PLACED.
- ALL STREET SIGNS SHALL BE IN CONFORMANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL FOR STREETS AND HIGHWAYS AND SHALL BE INSTALLED PRIOR TO RECORDING OF THE SUBDIVISION PLAT OR OCCUPYING OF THE FIRST RESIDENCE ON THE STREET, AND SHALL BE IN PLACE AT THE TIME OF BOND RELEASE. ALL SIGNS TO CONFORM TO KRS 67C.115 (2), CHAPTER 97 OF THE LOUISVILLE / JEFFERSON COUNTY METRO GOVERNMENT CODE OF ORDINANCES.

**MISCELLANEOUS**

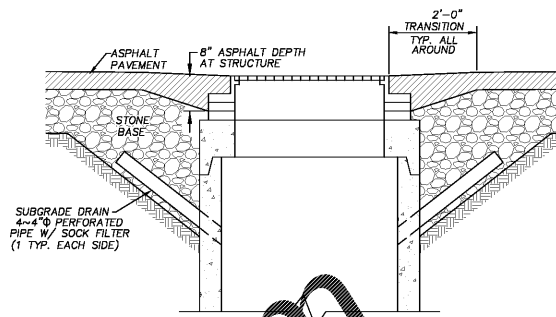
- WHEN ROCK IS ENCOUNTERED NEAR ROADBED ELEVATION, THE MATERIALS WILL BE EXCAVATED TO MINIMUM DEPTH OF TWO FEET (24 INCHES) BELOW SUBBASE ELEVATION, THE ROCK SURFACE SLOPED TO DRAIN, BACKFILLED WITH SELECT MATERIALS AND COMPACTED IN ACCORDANCE WITH THE KENTUCKY STANDARD SPECIFICATIONS FOR ROADS & BRIDGES. ANY OTHER UNSUITABLE MATERIALS ENCOUNTERED SHALL BE REMOVED TO A DEPTH AND WIDTH, AND BACKFILLED WITH ACCEPTABLE MATERIALS, AT THE DIRECTIONS OF THE ENGINEER OR METRO PUBLIC WORKS INSPECTOR.
- ALL EMBANKMENT BACKFILL AND SUBGRADE MATERIALS SHALL BE CONSTRUCTED AND COMPACTED TO 95% OF MAXIMUM DENSITY AND WITHIN PLUS 2 TO MINUS 4 PERCENT OF THE OPTIMUM MOISTURE CONTENT PER ASTM D-698.
- THE CONTRACTOR WILL UTILIZE A REGISTERED GEOTECHNICAL ENGINEER TO TEST, VERIFY AND REPORT TO PROVIDE SATISFACTORY ASSURANCE OF EMBANKMENT AND PAVEMENT STABILITY WHERE EMBANKMENT IS IN EXCESS OF FOUR FEET DEPTH. ALL EMBANKMENT SECTIONS IN EXCESS OF FOUR FEET DEPTH SHALL BE TESTED IN ONE FOOT LAYERS.
- ROADWAY/SITE EMBANKMENT SHALL BE CONSTRUCTED IN ACCORDANCE WITH KOOH STANDARD SPECIFICATION 207 (LATEST EDITION).
- ANY UNSUITABLE SOILS AND OTHER MATERIALS ENCOUNTERED DURING CONSTRUCTION OF THE ROADWAY SECTION WILL BE REMOVED TO THE DEPTH AND WIDTH SPECIFIED BY THE GEOTECHNICAL ENGINEER. THE EXCAVATION WILL BE BACKFILLED WITH SELECTED MATERIALS AND COMPACTED IN ACCORDANCE WITH EMBANKMENT SPECIFICATIONS.
- CONTRACTOR SHALL NOTIFY OWNER/DEVELOPER BEFORE CONSTRUCTION BEGINS OF THE GEOTECHNICAL ENGINEER CONTRACTED FOR COMPACTION TESTING.
- THE CONTRACTOR WILL PREGRADE THE ENTIRE ROADWAY SECTION WITHIN THE RIGHT-OF-WAY LIMITS, INCLUDING THE ROADWAY SLOPES, DRAINAGE DITCHES AND ROADWAY CROSS-SECTION AS SHOWN ON THE TYPICAL ROADWAY SECTION PRIOR TO PLACEMENT OF THE ROCK BASE COURSE.
- ACTIONS SHALL BE TAKEN TO MINIMIZE THE TRACKING OF MUD AND SOIL FROM CONSTRUCTION AREAS ONTO PUBLIC ROADWAYS. SOIL TRACKED ONTO THE ROADWAY SHALL BE REMOVED DAILY. ANY WORK PERFORMED BY A PUBLIC AGENCY TO CORRECT THE CONDITIONS WILL BE CHARGED TO THE CONTRACTOR.
- ALL FIRE HYDRANTS WILL BE INSTALLED IN ACCORDANCE WITH CURRENT ORDINANCES.
- IF ANY UTILITY LINES ARE ENCOUNTERED DURING CONSTRUCTION, EXTREME CAUTION SHOULD BE EXERCISED AND THE UTILITY COMPANY NOTIFIED IMMEDIATELY. ANY DAMAGES SHALL BE REPAIRED IMMEDIATELY AT THE DIRECTION OF THE UTILITY COMPANY, INCLUDING TEMPORARY AND PERMANENT WORK, AT NO ADDITIONAL EXPENSE TO OWNER/DEVELOPER.
- ALL EXISTING TRAFFIC, WARNING, AND REGULATORY SIGNS WILL BE LOCATED, MAINTAINED DURING ALL CONSTRUCTION ACTIVITIES, AND IMMEDIATELY REPLACED AFTER CONSTRUCTION IS COMPLETED BY THE CONTRACTOR.
- LANDSCAPING SHALL BE SELECTED AND PLACED IN SUCH A MANNER AS TO INSURE ADEQUATE AND SAFE "SIGHT DISTANCE" FOR MOTORISTS USING THE ROADS TO BE CONSTRUCTED WITHIN THIS PROJECT.
- THE LOCATION AND TYPE OF ANY TREES OR LANDSCAPING WITHIN THE ROADWAY RIGHT-OF-WAY WILL BE EVALUATED FOR ROADSIDE SAFETY AND SIGHT DISTANCE REQUIREMENTS BY METRO PUBLIC WORKS WHICH RESERVES THE RIGHT TO REMOVE THEM WITHOUT THE APPROVAL OF THE PROPERTY OWNER.
- A LANDSCAPING PLAN SHOWING THE TYPE AND LOCATION OF ALL PLANTINGS WITHIN THE ROAD RIGHT-OF-WAY SHALL BE PREPARED AND SUBMITTED TO THE DEPARTMENT OF PUBLIC WORKS FOR APPROVAL. THE ULTIMATE (OR NATURAL) HEIGHT AND SPREAD OF THE VARIOUS TYPE OF PLANTINGS SHALL BE SPECIFIED ON THE PLAN.
- CONSTRUCTION AND EMBANKMENT PERMIT MAY BE REQUIRED BY METRO PUBLIC WORKS PRIOR TO CONSTRUCTION AND SHALL BE FOR ROADWAY REPAIRS ON SURROUNDING ACCESS ROADS TO THE SITE DUE TO DAMAGES CAUSED BY CONSTRUCTION TRAFFIC ACTIVITIES.

**APPLICABLE M.S.D. STANDARD DRAWINGS**

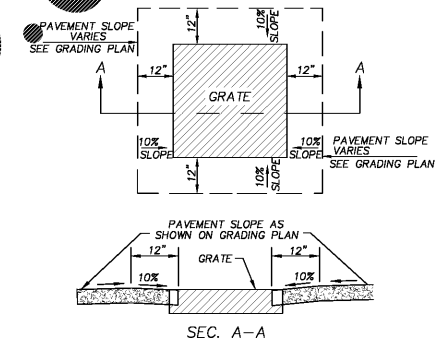
ITEM	DRAWING NUMBER
CATCH BASIN TYPE 2	DC-02-02
DOUBLE FRAME & GRATE TYPE 3-L	DC-06-04
12"x12" YARD DRAIN	DI-07-00
PRECAST CATCH BASIN SLAB TYPE 3	DI-05-02
CRUSHED STONE CRADLE	GC-01-01
PRECAST MANHOLE TYPE 2	GM-02-04
STANDARD MANHOLE FRAME AND LID	GM-05-03
CURB TO CATCH BASIN TRANSITION	PC-03-01
STABILIZED CONSTRUCTION ENTRANCE	ER-01-03
SILT FENCE	EF-09-02

**APPLICABLE L.M.P.W. STANDARD DRAWINGS**

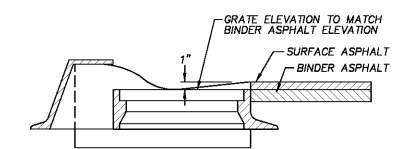
ITEM	PLAN NUMBER
HEADER CURB	410A
DEPRESSED ROLL CURB	410A
CONCRETE SIDEWALK	420
CURB RAMPS	425B
DIRECTIONAL CURB RAMP	425E



**1 TYPICAL SECTION AT ALLEY INLETS**  
CD100 NIS

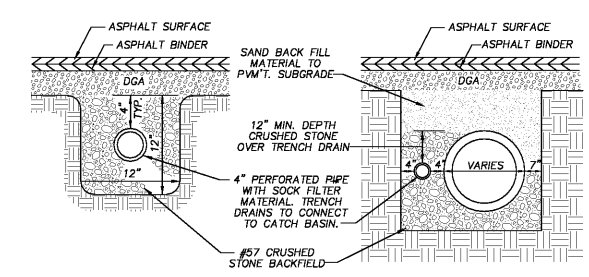


**2 GRADING AROUND GRATES IN ALLEYS**  
CD100 NIS



NOTES:  
1) TRANSITION CURB TO MATCH CATCH BASIN CURB IRON SHAPE IN 2 LINEAR FEET.  
2) TAPER SURFACE ASPHALT TO MATCH GRATE ELEVATION OVER 2 INCHES.  
3) CONSTRUCT EXPANSION JOINTS WITHIN 2 FEET OF BOTH SIDES OF THE CURB IRON.

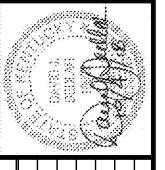
**3 CURB INLET**  
CD100 NIS



**4 SUBGRADE DRAIN**  
CD100 NIS

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LOUISVILLE, KENTUCKY 40202  
(502) 584-6271



NO.	REVISION	DATE
1.	MOD AND CLIENT COMMENTS	8/27/18

SHEET TITLE: NOTES & DETAILS  
PROJECT TITLE: LYRIC AT NORTON COMMONS (LOT N701)  
OWNER / DEVELOPER: BRISTOL LYRIC PARTNERS LLC  
381 HALLORY STATION ROAD, STE. 204  
FRANKLIN, TENNESSEE 37067  
(615) 258-9000

JOB NO.	3061
SCALE:	AS SHOWN
DATE:	8/31/18
DRAWING NO.	<b>CD100</b>
SHEET	2 OF 10