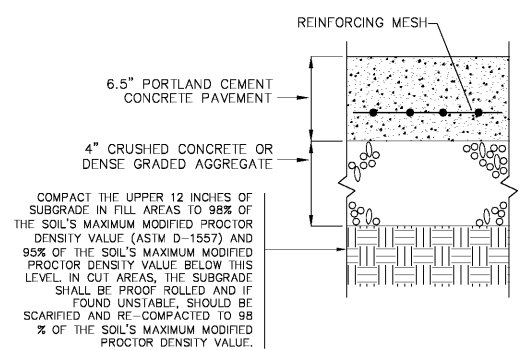


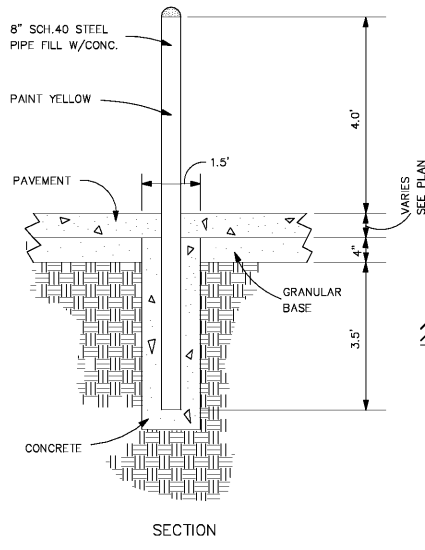
RETAIL HEAVY DUTY PAVING SECTION



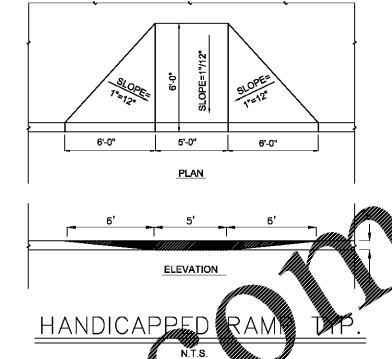
RETAIL HEAVY DUTY CONCRETE PAVING SECTION

PAVING SECTIONS (NOT TO SCALE)

NOTE: THE DETAILED PAVEMENT DESIGN NOTED ABOVE IS MERELY A DEPICTION OF THE PAVEMENT DESIGN RECOMMENDATIONS INCLUDED IN THE ORIGINAL GEOTECHNICAL REPORT RECOMMENDATIONS FOR THE SITE. THE PAVEMENT DEPICTION ABOVE IS SHOWN ON THIS DRAWING FOR THE CONVENIENCE OF THE CONTRACTOR DURING CONSTRUCTION. THE CONTRACTOR SHOULD REFER TO ALL RECOMMENDATIONS INCLUDED IN THE GEOTECHNICAL REPORT AND ALL ADDENDUMS, AND ALL CONSTRUCTION SHOULD COMPLY WITH THOSE RECOMMENDATIONS. HAINES, GIPSON & ASSOC. INC. IS NOT RESPONSIBLE FOR THE PAVEMENT DESIGN NOTED ABOVE.

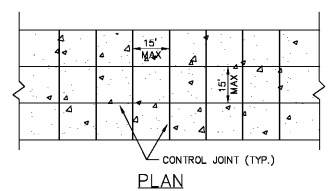


GUARD POST DETAIL

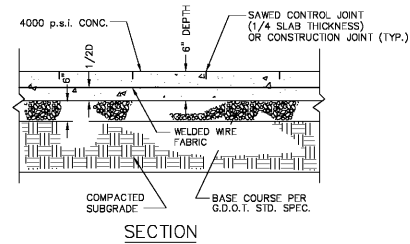


HANDICAPPED RAMP TOP

- NOTES:
1. SURFACES ALONG ACCESSIBLE ROUTES SHALL BE SLIP RESISTANT.
 2. MAXIMUM SLOPE OF FLARE SHALL BE 1V:12H.
 3. CURBS SHALL HAVE DETECTABLE WARNING TEXTURE, INCLUDING THE FULL WIDTH AND DEPTH OF THE CURB RAMP, INCLUDING FLARES. (DETECTABLE WARNING SHALL CONSIST OF RAISED TRUNCATED DOMES WITH A DIAMETER OF "NOMINAL 0.9", A HEIGHT OF "NOMINAL 0.2" AND A CENTER-TO-CENTER SPACING OF "NOMINAL 0.8".) MATERIALS USED TO PROVIDE CONTRAST SHALL BE AN INTEGRAL PART OF THE WALKING SURFACE.)
 4. BUILT-UP CURB RAMP SHALL BE LOCATED SO THAT THEY DO NOT PROJECT INTO VEHICULAR TRAFFIC LANES.
 5. POUR RAMP AND CURB SECTION INTEGRAL (TYPICAL).



PLAN

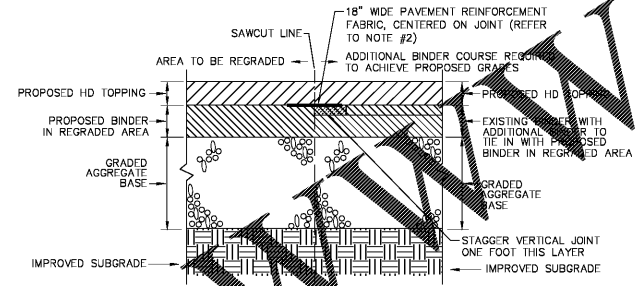


SECTION

CONCRETE PAVING DETAIL (NOT TO SCALE)

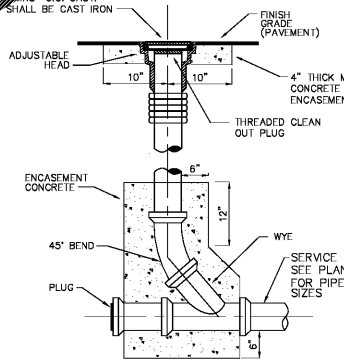
NOTE: THIS PAD DETAIL TO BE USED FOR DUMPSTER, TRANSFORMER AIR COMPRESSOR PADS, AND ROLL-UP DOOR INSTALLATIONS AND DRIVEWAYS TO ROLL UP DOORS

- NOTES:
1. LONGITUDINAL AND TRANSVERSE JOINTS SHALL BE SAWS AS INDICATED.
 2. THE JOINT SEAL MANUFACTURER'S SPECIFICATIONS SHALL BE COMPATIBLE WITH THE JOINT CONFIGURATION.
 3. FOR ALL JOINTS THE BACKER ROD MATERIAL SHALL BE COMPATIBLE WITH THE COLD Poured SEALANT AND SLIGHTLY OVERSIZED TO PREVENT MOVEMENT DURING THE JOINT SEALANT OPERATION.
 4. THE WIDTH OF ALL JOINTS SHALL BE CORRECTED FOR 68 DEGREE F.
 5. EXPANSION JOINTS SHALL BE USED AT ALL LOCATIONS WHERE PAVEMENT ABUTS A PERMANENT STRUCTURE OR AS DIRECTED BY THE ENGINEER.
 6. DOWELS AND TIE BARS FOR CONSTRUCTION JOINTS SHALL BE CAST IN PLACE.
 7. CONCRETE PAVEMENT SHOULD HAVE A MINIMUM 28-DAY COMPRESSIVE STRENGTH OF 4000 PSI WITH A MODULUS OF RUPTURE OF 630 PSI.
 8. CONSULT ON-SITE GEOTECHNICAL ENGINEER ON WWF PLACEMENT.

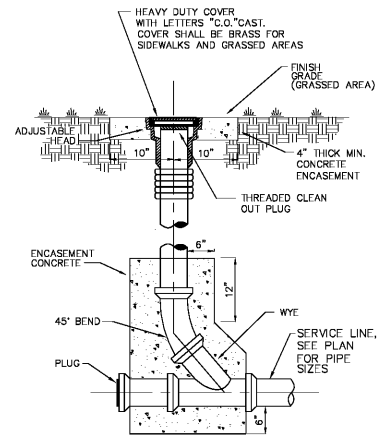


PAVEMENT MATCHING & JOINT REINFORCEMENT SECTION DETAIL (NOT TO SCALE)

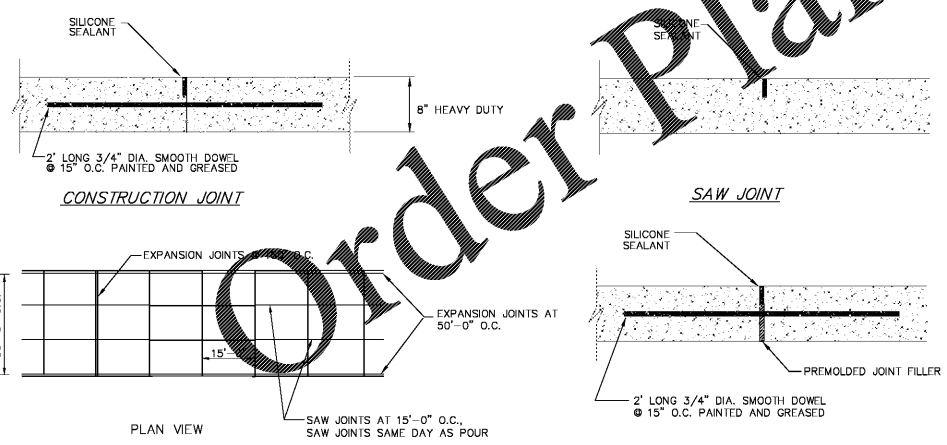
- NOTES:
1. SEE "PAVEMENT SECTIONS" DETAIL FOR ACTUAL PAVEMENT SECTIONS.
 2. WHEN TRYING TO MATCH EXISTING LANE PAVEMENT, MILL EXISTING LANE ONE (1) FOOT WIDE TO DEPTH OF ADJOINING LAYER TO BE PLACED. COST OF MILLING FOR THIS WORK TO BE INCLUDED IN THE UNIT PRICE BID FOR PAVEMENT REINFORCING FABRIC.



CLEAN-OUT DETAIL IN PAVED AREAS N.T.S.



CLEAN-OUT DETAIL IN GRASSED AREAS N.T.S.

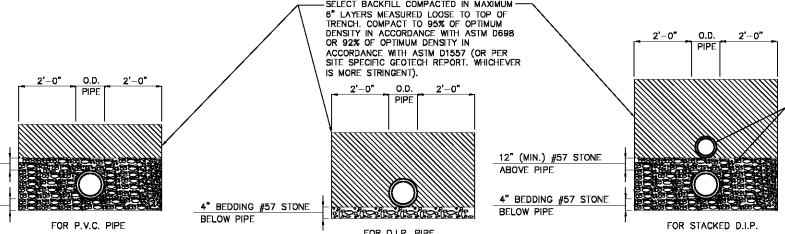


CONCRETE PAVING

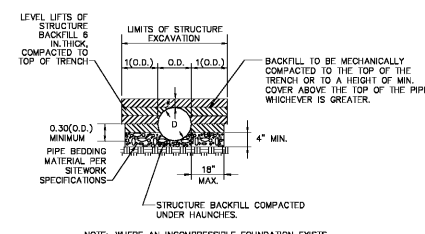
EXPANSION JOINT

CONCRETE PAVING JOINT DETAILS (NOT TO SCALE)

NOTE: PROVIDE EXPANSION JOINT WHERE CONC. PAVEMENT AND CURB AND GUTTER MEET OR CONSTRUCT MONOLITHIC CURB AND GUTTER WITH CONCRETE PAVEMENT.



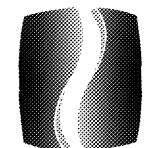
UTILITY SERVICE TRENCHING & BACKFILLING (NOT TO SCALE)



- NOTES:
- NOTE: WHEN AN INCOMPRESSIBLE FOUNDATION EXISTS, EXCAVATE AN ADDITIONAL 6"
 - NOTE: PIPE SHALL BE BEDDED IN A FOUNDATION SHAPED TO FIT THE LOWER PART OF THE PIPE.
 - NOTE: MINIMUM COVER OF STRUCTURE BACKFILL IS D/8 OR 12 IN. WHICHEVER IS GREATER.
 - NOTE: BACKFILL MATERIAL TO BE PER SITWORK SPECIFICATIONS.
 - NOTE: UNDERGROUND DETENTION PIPE STONE BEDDING TO HAVE A MINIMUM DEPTH TO THE SPRING LINE OF PIPE.

STORM SEWER TRENCHING & BACKFILLING (NOT TO SCALE)

CONSTRUCTION DETAILS



PHILLIPS architects + engineers

770.394.1616



SEAL

5/30/19

#	DATE	DESCRIPTION
1	05/30/2019	PERMIT SUBMITTAL

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PROJECT: THE PROMENADE

D'BERVILLE, MS.



PHILLIPS JOB NUMBER: 1925401

ISSUE DATE: 05/30/2019

DRAWN BY/CHECKED BY:

DRAWING TITLE: CONSTRUCTION DETAILS

SHEET NUMBER: C-5.0

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