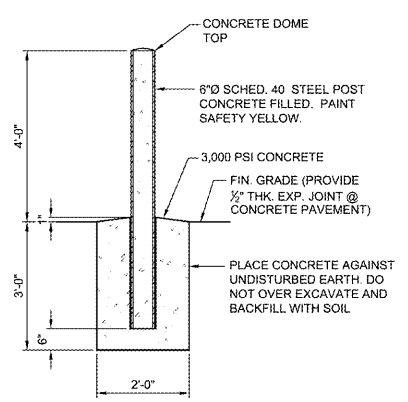
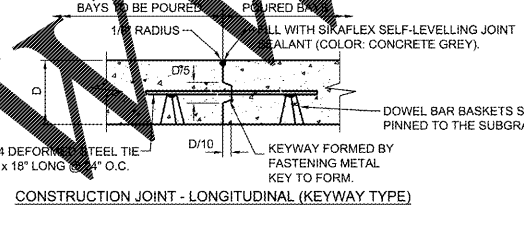
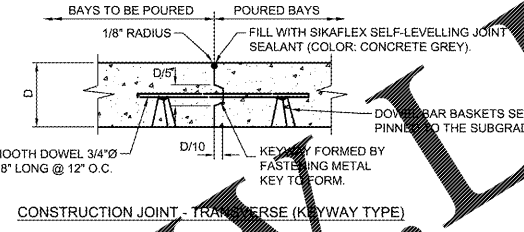
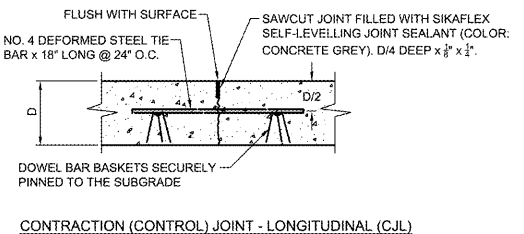
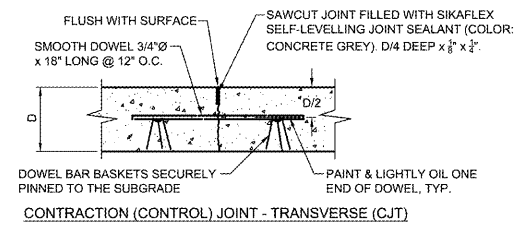


TYPICAL SECTION THRU ASPHALT PAVEMENT AND BASE (HEAVY-DUTY)
NO SCALE



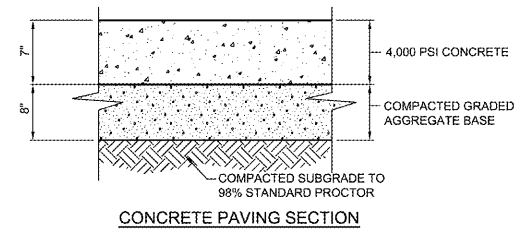
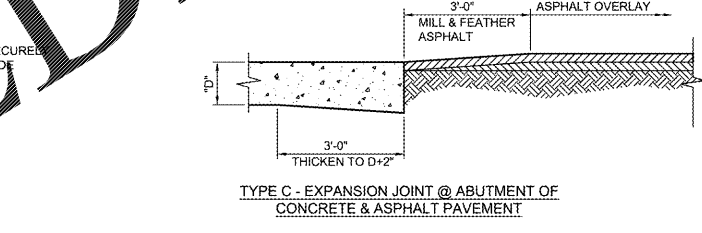
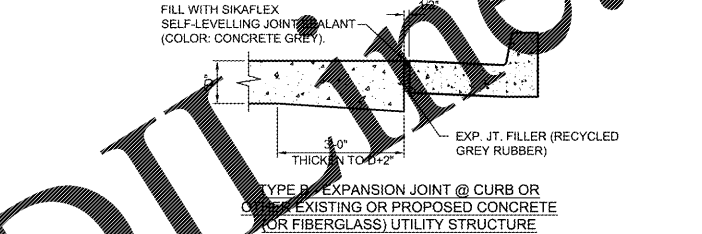
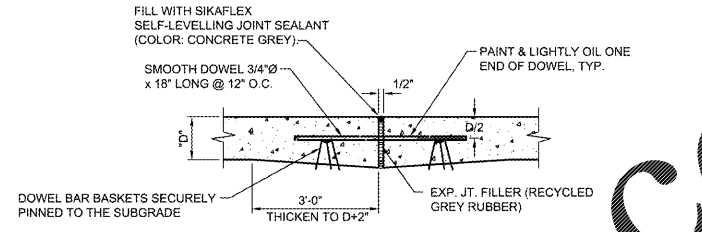
PIPE BOLLARD DETAIL
NO SCALE



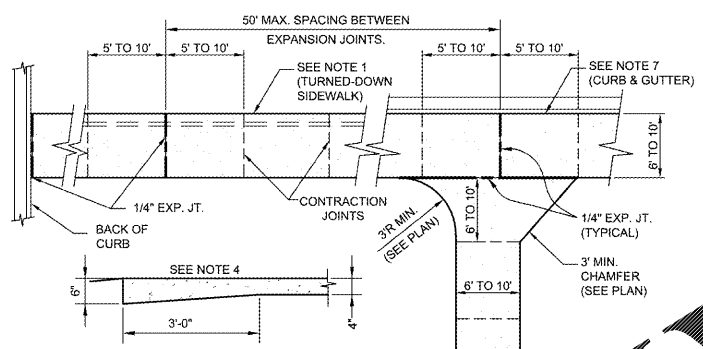
LEGEND

---	CONSTRUCTION JOINTS (LONGITUDINAL) (COJL)
---	CONSTRUCTION JOINTS (TRANSVERSE) (COJT)
---	CONTRACTION JOINTS (LONGITUDINAL) (CJL)
---	CONTRACTION JOINTS (TRANSVERSE) (CJT)
---	EXPANSION JOINTS (EJTA, EJTB, EJTC)

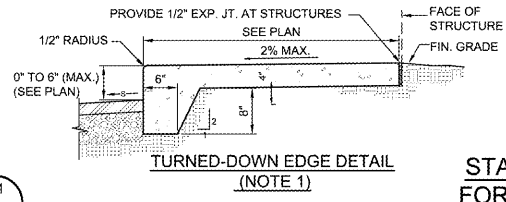
* NOTE: SEE JOINTING PLAN ON C101 FOR LOCATIONS.



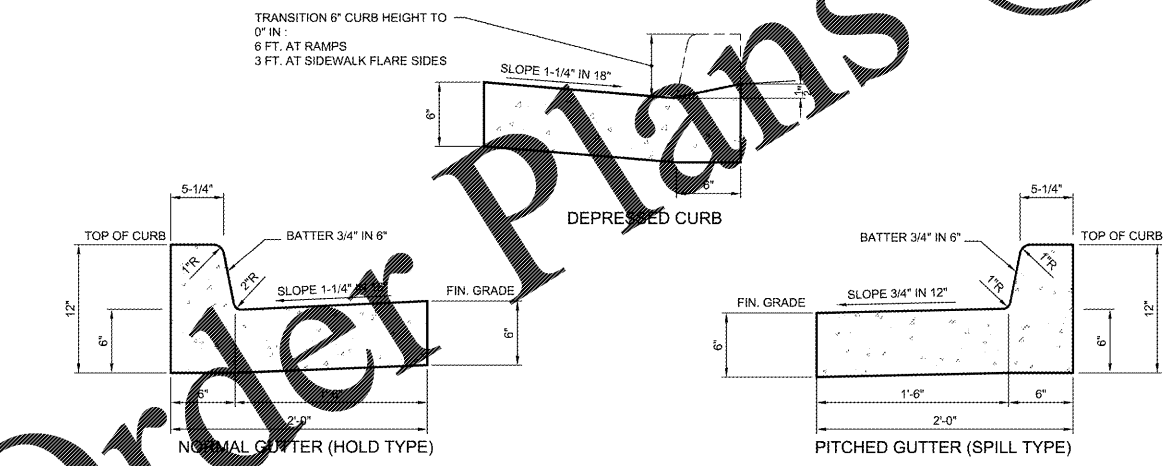
- NOTES:**
- ALL SIDEWALKS SHALL BE 4" THICK, EXCEPT WHERE SHOWN WITH TURNED-DOWN EDGE (SEE "TURNED-DOWN EDGE SIDEWALK" DETAIL BELOW). CONTRACTION JOINTS SHALL BE PLACED AT 6' INTERVALS FOR 6' SIDEWALKS AND 5' INTERVALS FOR 10' SIDEWALKS W/ A LONGITUDINAL JOINT IN THE CENTER OF THE 10' WIDE SIDEWALKS; CONTRACTION JOINTS SHALL BE 1" DEEP AND EDGED WITH 1/8" RADIUS.
 - CONSTRUCT 1/4" EXP. JOINT MAXIMUM 50 FT. SPACING BETWEEN JOINTS.
 - WHEN THE SIDEWALK ABUTS A ROADWAY OR WHERE THERE IS NO CURB, THICKEN THE SIDEWALK TO 6" IN 3'-0" WHERE SIDEWALKS ARE USED IN PARKING LOTS IN LIEU OF CURB & GUTTER SEE "TURNED-DOWN EDGE SIDEWALK" DETAIL.
 - INSTALL 1/2" EXP. MATERIAL WHERE CONC. SIDEWALKS ABUT CONCRETE STRUCTURES.
 - A LAYER OF 15# FELT IS REQUIRED BETWEEN WALK AND ADJACENT PARALLELING CONCRETE CURB.
 - CONSTRUCT WHEELCHAIR RAMP AND DETECTABLE WARNING PER GEORGIA D.O.T. AND ADA STANDARDS (SEE SITE PLAN FOR TYPE).
 - ALL CONCRETE SHALL BE 3,000 PSI.
 - SIDEWALK CROSS SLOPE NOT TO EXCEED 2.0%.
 - CONCRETE SIDEWALK SHALL HAVE POLYPROPYLENE FIBER REINFORCING AT A RATE OF 1.5#/CY.
 - PROVIDE 1/2" EXP. JOINT FILLER BY REFLEX RUBBER (COLOR CONCRETE GRAY) PER ASTM D175 & ASTM D1752 AT ALL BLDG. FOUNDATIONS, DRAINAGE STRUCTURES, AREA TRANSITIONS AND WHERE NOTED ON DRAWINGS.



STANDARD JOINT LAYOUT FOR CONCRETE SIDEWALK
NO SCALE



TURNED-DOWN EDGE DETAIL (NOTE 1)



CONCRETE CURB & GUTTER DETAIL
NO SCALE

- CONCRETE PAVING NOTES:**
- PROVIDE 1/2" EXP. JOINT FILLER TO BE GREY RECYCLED RUBBER PER ASTM D175 & ASTM D1752 AT ALL BLDG. FOUNDATIONS, DRAINAGE STRUCTURES, AREA TRANSITIONS, AND WHERE NOTED IN DRAWINGS.
 - THICKENED SLAB EDGE 2" IN 36" AT EDGE OF PAVING.
 - SEE PLAN FOR JOINTING LAYOUT. IF NOT SHOWN, MAXIMUM JOINT SPACING (FEET) SHALL BE 2-1/2 x PAVEMENT THICKNESS (INCHES) CONTRACTOR SHALL ADJUST JOINT LOCATIONS TO LINE UP WITH CORNERS OF STRUCTURES. IF NOT SHOWN, A JOINTING PLAN SHALL BE PREPARED BY THE CONTRACTOR IN ACCORDANCE WITH CONSTRUCTION PRACTICES SET FORTH BY THE PORTLAND CEMENT ASSOCIATION AND APPROVED BY THE ENGINEER BEFORE PAVING BEGINS.
 - ALL CONC. SHALL BE 4,000 PSI.
 - CONCRETE PAVEMENT SHALL HAVE POLYPROPYLENE FIBER REINFORCING AT A RATE OF 1.5#/CY.
 - SAWCUT JOINTS SHALL BE PERFORMED W/ A SOFT CUT SAWBLADE IMMEDIATELY AFTER 1ST TROWEL. IN AREAS WHERE OBSTRUCTIONS PREVENT SAWCUTTING, A TOOLED JOINT SHALL BE UTILIZED & TIED INTO THE SAWCUT JOINT.
 - WHERE PAVEMENT PLACEMENT WILL BE INTERRUPTED BY 30 MINUTES OR MORE, A CONSTRUCTION JOINT SHALL BE USED.
 - CONTRACTOR SHALL INSURE THAT DOWELS & TIE BARS ARE FIXED SUCH THAT THEY ARE PARALLEL TO THE SURFACE OF THE CONCRETE & PERPENDICULAR TO THE JOINT DURING & AFTER PLACEMENT OF CONCRETE.

POLYPROPYLENE FIBER REINFORCED CONCRETE PAVEMENT DETAILS
NO SCALE

CLIENT: AUGUSTA, GA
355 TELFAIR STREET, AUGUSTA, GA 30901
PROJECT NAME: FIRE STATION #20
PROJECT LOCATION: 2820 OLD HIGHWAY 1, HEPHIZIBAH, GEORGIA 30815

NO.	DATE	BY	DESCRIPTION
0	12/21/18	DRWN	ISSUED FOR BID

PROJECT NO: 3042.1604
DRAWN BY: DRWN
CHECKED BY: CHK
DATE: 8/10/18

CONSTRUCTION DETAILS 1 OF 7

SCALE: 1" = 20"
DRAWING NO: C-601
REV: 0

Order Plans

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