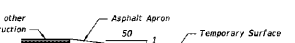


PROJECT INFORMATION SIGN NOTES:

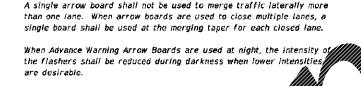
- Road designation should be the most common designation (i.e. 1-Interstate, SR-State Road or US).
- Traffic text on signs indicate variable information specific to the project.
- See Sheet 5 for Typical Foundation Details and Post and Foundations Table.

LAST REVISION 11/01/17	DESCRIPTION: FDOT	FY 2018-19 STANDARD PLANS	GENERAL INFORMATION FOR TRAFFIC CONTROL THROUGH WORK ZONES	INDEX 102-600	SHEET 6 of 12
------------------------	-------------------	---------------------------	--	---------------	---------------

MANHOLES/CROSSWALKS/JOINTS
 Manholes extending 1' or more above the travel lane and crosswalks having an uneven surface greater than 1/4" shall have a temporary asphalt apron constructed as shown in the diagram below.



ADVANCE WARNING ARROW BOARDS
 An arrow board in the arrow or chevron mode shall be used only for stationary or moving lane closures on multilane roadways.

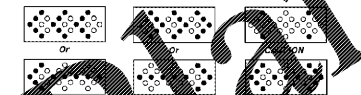


PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS)
 The PCMS can be used to:

- Supplement standard signing in construction or maintenance work zones.
- Reinforce static advance warning signs.
- Provide motorist with updated variable information.

TRUCK/TRAILER-MOUNTED ATTENUATORS
 Truck/trailer-mounted attenuators (TMA) can be used for moving operations and short-term stationary operations.

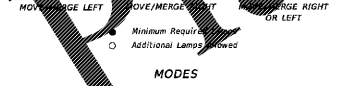
REMOVING PAVEMENT MARKINGS
 Existing pavement markings that conflict with temporary work zone delineation shall be removed by any method approved by the Engineer.



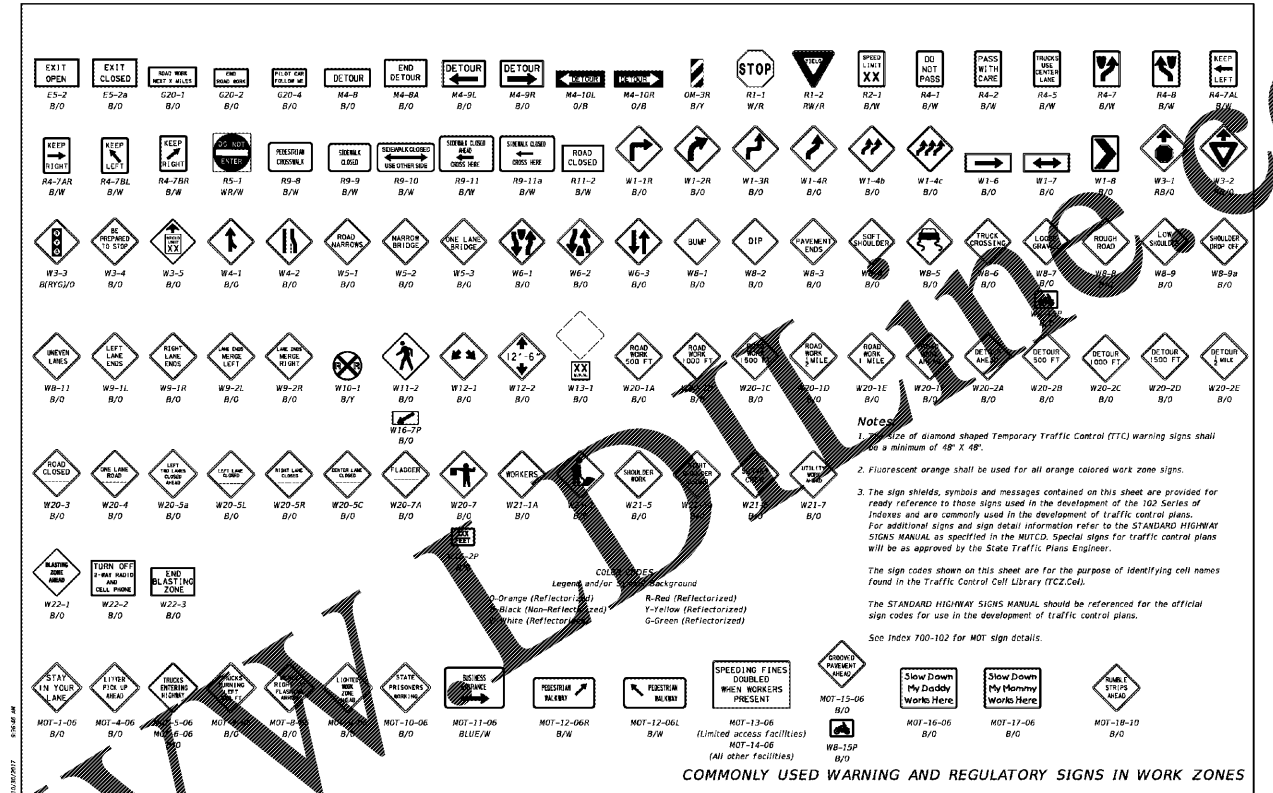
CHANNELIZING DEVICES
 Channelizing devices for work zone traffic control shall be as prescribed in Part VI of the MUTCD.

CHANNELIZING DEVICE CONSISTENCY
 Barricades, vertical panels, cones, tubular markers and drums shall not be intermixed within either the lateral transition or within the tapered alignment.

SIGNALS
 Existing traffic signal operations that require modification in order to carry out work zone traffic control shall be included in the TCP and approved by the District Traffic Operations Engineer.



LAST REVISION 11/01/17	DESCRIPTION: FDOT	FY 2018-19 STANDARD PLANS	GENERAL INFORMATION FOR TRAFFIC CONTROL THROUGH WORK ZONES	INDEX 102-600	SHEET 8 of 12
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Notes:

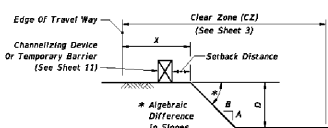
- Size of diamond shaped Temporary Traffic Control (TTC) warning signs shall be a minimum of 48" x 48".
- Fluorescent orange shall be used for all orange colored work zone signs.
- The sign shields, symbols and messages contained on this sheet are provided for ready reference to those signs used in the development of the 102 Series of Indexes and are commonly used in the development of traffic control plans.

LAST REVISION 11/01/17	DESCRIPTION: FDOT	FY 2018-19 STANDARD PLANS	GENERAL INFORMATION FOR TRAFFIC CONTROL THROUGH WORK ZONES	INDEX 102-600	SHEET 7 of 12
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DROP-OFF CONDITION NOTES

- These conditions and treatments can be applied only in work areas that fall within a properly signed work zone.
- When drop-offs occur within the clear zone due to construction or maintenance activities, protection devices are required (See Table 1). A drop-off is defined as a drop in elevation parallel to the adjacent travel lanes, greater than 2' with slope (AB) steeper than 1:4. In super-elevated sections, the algebraic difference in slopes should not exceed 0.25 (See Drop-off Condition Detail).

- Drop-offs may be mitigated by placement of slopes with optional base material per Specifications Section 205. Slopes shallower than 1:4 may be required to avoid algebraic difference in slopes greater than 0.25. Include the cost for the placement and removal of the material in Maintenance of Traffic, LSD. Use of this treatment in lieu of a temporary barrier is not eligible for CSIP consideration. Conduct daily inspections for deficiencies related to erosion, excessive slopes, ruts or other adverse conditions. Repair any deficiencies immediately.
- For setback barriers, refer to the Index or Approved Products List (APL) drawing of the selected barrier.
- For Conditions 1 and 3 provided in Table 1, an isolated drop-off condition less than 100 feet in length that is created and restored within the same work period will not be subject to the use of temporary barriers; however, channelizing devices will be required.
- When permanent curb heights are $\geq 6"$, no channelizing device will be required. For curb heights $< 6"$, see Table 1.



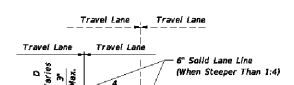
Drop-off Condition Detail

Table 1 Drop-off Protection Requirements

Condition	X (ft)	D (ft.)	Device Required
1	0-12	> 3	Temporary Barrier
2	$> 12-CZ$	> 3 to ≤ 3	Channelizing Device
3	0-42	> 3	Temporary Barrier
4	Removal of Bridge or Retaining Wall Barrier	-	Temporary Barrier
5	Removal of portions of Bridge Deck	-	Temporary Barrier

TRAVEL LANE TREATMENT FOR MILLING OR RESURFACING NOTES

- This treatment applies to resurfacing or milling operations between adjacent travel lanes.
- Whenever there is a difference in elevation between adjacent travel lanes, the WB-11 sign with "UNEVEN LANE" is required at intervals of 1/2 mile maximum.
- If D is 1/2' or less, no treatment is required.
- Treatment allowed only when D is 3' or less.
- If the slope is steeper than 1:4 (not to be steeper than 1:1), the R4-1 and M07-1-06 signs shall be used as a supplement to the WB-11; this condition should never exceed 3 miles in length.



TRAVEL LANE TREATMENT FOR MILLING OR RESURFACING DETAIL

PEDESTRIAN WAY DROP-OFF CONDITION NOTES

- A pedestrian way drop-off is defined as:

- a drop in elevation greater than 10" that is closer than 2' from the edge of the pedestrian way
- a slope steeper than 1:2 that begins closer than 2' from the edge of the pedestrian way when the total drop-off is greater than 60"

DROP-OFFS IN WORK ZONES

LAST REVISION 11/01/17	DESCRIPTION: FDOT	FY 2019-20 STANDARD PLANS	GENERAL INFORMATION FOR TRAFFIC CONTROL THROUGH WORK ZONES	INDEX 102-600	SHEET 9 of 12
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STATE OF FLORIDA AUTHORIZATION NUMBER 8370
 PLANS PREPARED BY INGENIUM ENTERPRISES FORMERLY GRIFFAIL CRAWFORD

PANDA EXPRESS/CFT PLAZA 3668 GULF BREEZE PARKWAY GULF BREEZE, FLORIDA

CLIENT: CFT IV DEVELOPMENTS, LLC
 1120 N. TOWN CENTER DR., SUITE 150, LAS VEGAS, NV 89144, PHONE: (626) 799-8594

REVISION HISTORY

NO.	DATE	DESCRIPTION
1	11/07/17	Issue for Bid
2	11/07/17	Issue for Bid
3	11/07/17	Issue for Bid
4	11/07/17	Issue for Bid
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HARDSCAPE DETAILS VII

C08.8
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