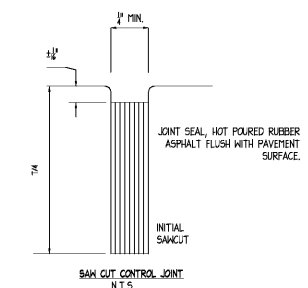
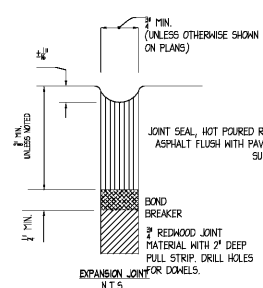
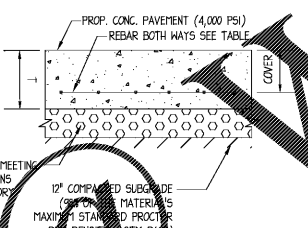


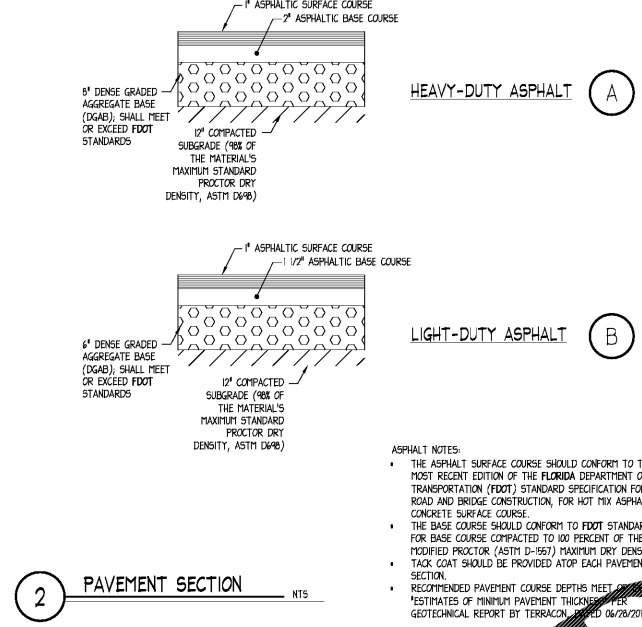
- NOTES:**
- REINFORCING STEEL BAR SIZE/SPACING SPECIFICATIONS IN GEOTECH REPORT SHALL SUPERSEDE ABOVE TABLE.
 - REINFORCING STEEL SIZE/SPACING IS BASED ON MIN. 40,000 PSI TENSILE STRENGTH REINFORCING STEEL AS SHOWN.
 - CONCRETE PAVING MIX DESIGN SHALL HAVE MINIMUM 4000 PSI COMPRESSIVE STRENGTH AT 28 DAYS. GEOTECHNICAL REPORT CONCRETE PAVING MIX DESIGN SHALL SUPERSEDE VALUES HEREIN.
 - MAXIMUM JOINT SPACING SHALL BE PER JOINT LAYOUT PLAN (IF PROVIDED) BUT SHALL NOT EXCEED VALUES IN TABLE.
 - MAXIMUM JOINT SPACING IN GEOTECHNICAL REPORT SHALL SUPERSEDE VALUES IN ABOVE TABLE.
 - USE STATE DOT SUBGRADE UNLESS OTHERWISE SPECIFIED BY GEOTECHNICAL REPORT.
 - ALL JOINTS IN PAVING SHALL BE REFLECTED IN CURBING AND SHALL HAVE ALL THEIR RESPECTIVE JOINTING MATERIALS PRESENT (I.E. EXPANSION JOINTS SHALL HAVE THEIR RESPECTIVE FILLER BOARD AND CAULK REPLACED).
 - CURB EXPANSION JOINTS - IF THERE IS AN EXPANSION JOINT IN THE PAVING, THE EXPANSION JOINT MUST FOLLOW THROUGH THE CURB. THE REINFORCING STEEL MUST ALSO BE CUT AT THE EXPANSION JOINT AND NOT ALLOWED TO RUN THROUGH THE JOINT CONTINUOUSLY. A SAW CUT EXPANSION JOINT IS NOT ACCEPTABLE BECAUSE NORMAL EXPANSION AND CONTRACTION WILL CAUSE THE CONCRETE TO PUSH AGAINST THE TWO SECTIONS AND ONE SIDE WILL EVENTUALLY FAIL. IF AN EXPANSION JOINT IS LEFT OUT AND MUST BE SAW CUT IN, THE CURB SHOULD BE CUT TWICE AND A 1/2" PIECE OF CONCRETE IS REMOVED. IN ALL CASES THE JOINT SHOULD BE CAULKED WITH NP.
 - CONCRETE TOUCHING THE BACK OF CURBS- ANY CONCRETE THAT TOUCHES THE BACK OF A CURB INCLUDING SIDEWALKS, ISLAND NOSINGS AND PAYPHONE PADS SHALL BE ISOLATED FROM THE CURB USING 1/2" BLACK ASPHALT IMPREGNATED FIBERBOARD. CONTRACTOR SHALL USE A REMOVABLE STRIP OR A ZIP-STRIP AND SEAL THE JOINT WITH SLL. THE ONLY EXCEPTION IS IF THE ISLAND NOSINGS ARE POURED MONOLITHICALLY WITH THE CURB AND PARKING LOT.
 - CURBS AT THE BUILDING FOUNDATION- IF A CURB TOUCHES THE BUILDING FOUNDATION, IT NEEDS TO BE ISOLATED WITH EXPANSION JOINT MATERIAL JUST LIKE THE PAVING. IF AN EXPANSION JOINT IS LEFT OUT AND MUST BE SAW CUT IN, A 1/2" PIECE OF CONCRETE SHOULD BE REMOVED. THE JOINT SHOULD BE CAULKED WITH NP.
 - EXPANSION JOINTS AT ISLAND NOSINGS- IF THE ISLAND NOSINGS ARE POURED MONOLITHICALLY WITH THE CURB AND PARKING LOT, THEN PAVING EXPANSION JOINTS SHOULD CONTINUE THROUGH THE NOSINGS.



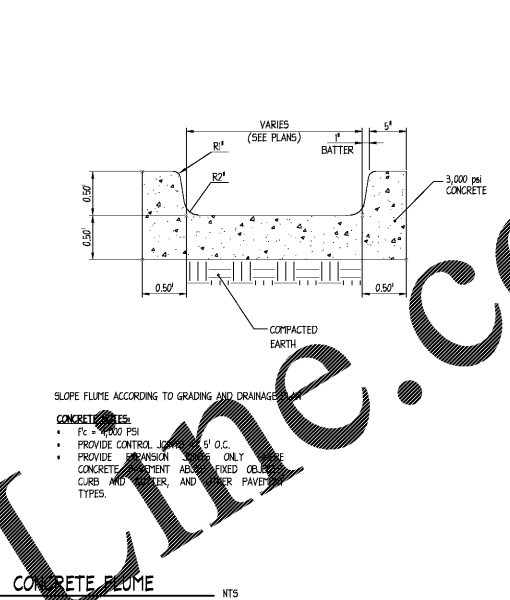
CONCRETE SECTION DESIGNATION	SLAB THICKNESS (IN.)	COVER (IN.) (2" MIN)	MAX. EXPANSION JOINT SPACING (FT.)	60,000 PSI STEEL REINFORCING STEEL BAR SIZE & SPACING
TYPE 'A'	5	3	12	#3 @ 24" C-C
TYPE 'B'	6	4	12	#3 @ 24" C-C
TYPE 'C'	7	4 1/2	15	#3 @ 24" C-C



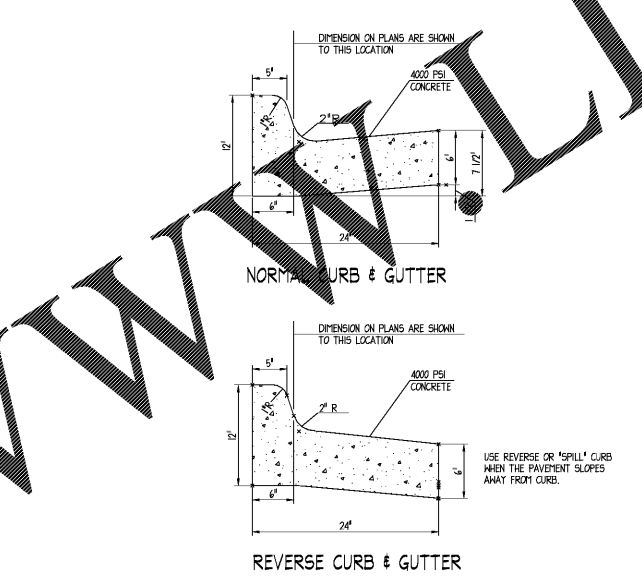
1 CONCRETE SECTION N.T.S.



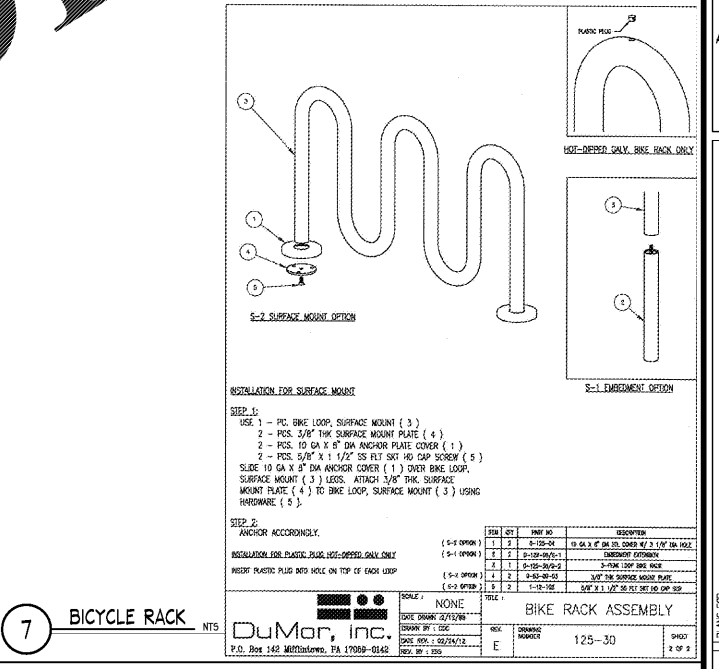
2 PAVEMENT SECTION N.T.S.



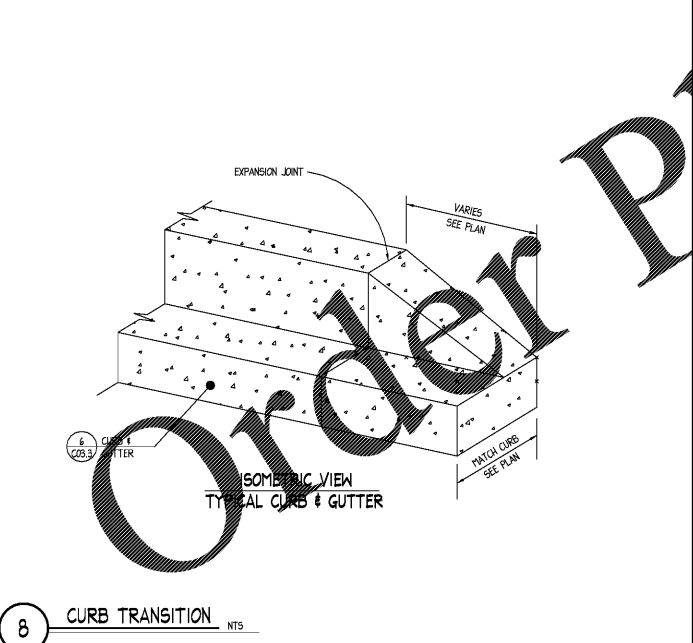
3 CONCRETE CURB N.T.S.



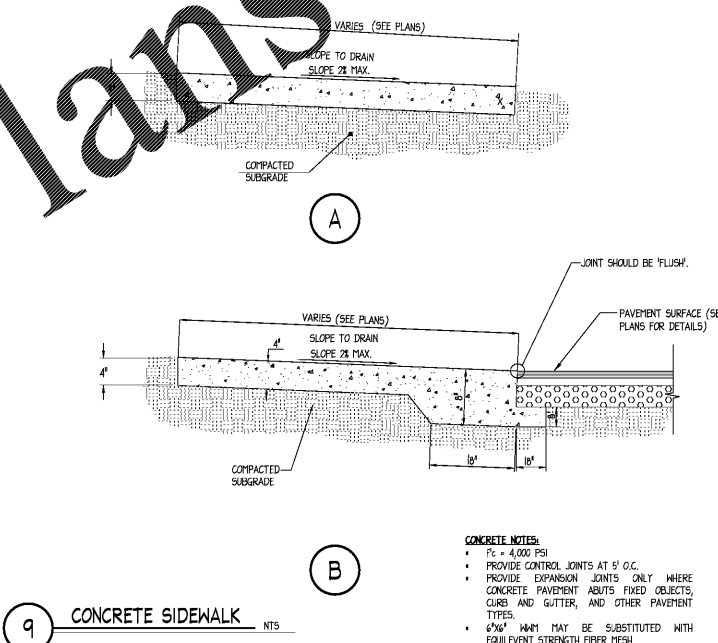
6 24" CURB & GUTTER N.T.S.



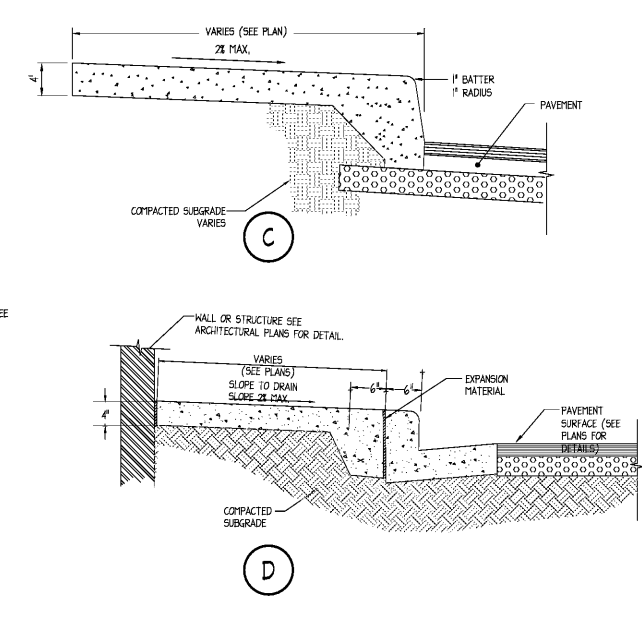
7 BICYCLE RACK N.T.S.



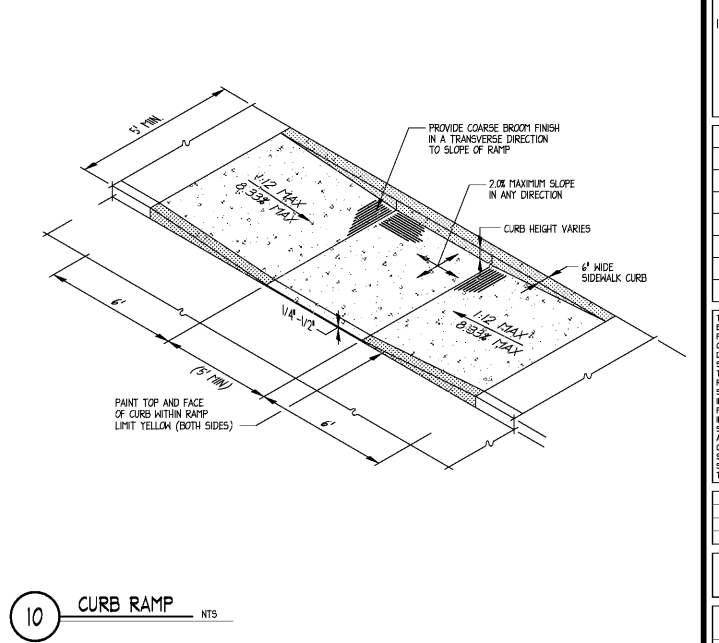
8 CURB TRANSITION N.T.S.



9 CONCRETE SIDEWALK N.T.S.



10 CURB RAMP N.T.S.



10 CURB RAMP N.T.S.

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JAMES D. NEFF
REGISTERED PROFESSIONAL ENGINEER
STATE OF FLORIDA
No. 92683
EXPIRES 12/31/16

THIS ITEM HAS BEEN DIGITALLY SIGNED AND SEALED BY JAMES D. NEFF, PE ON THE DATE ADJACENT TO THE SEAL.

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CFT NV DEVELOPMENTS, LLC
1120 N. TOWN CENTER DR., SUITE 150
LAS VEGAS, NV 89144
PHONE: (626) 799-8599

REVISION HISTORY

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PROJ # 17056
DWG NAME 17056 CURB.DWG
ISSUE DATE 11/2/2016
PROJ MGR EF

HARDSCAPE DETAILS II

C09.3
SHEET NUMBER

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