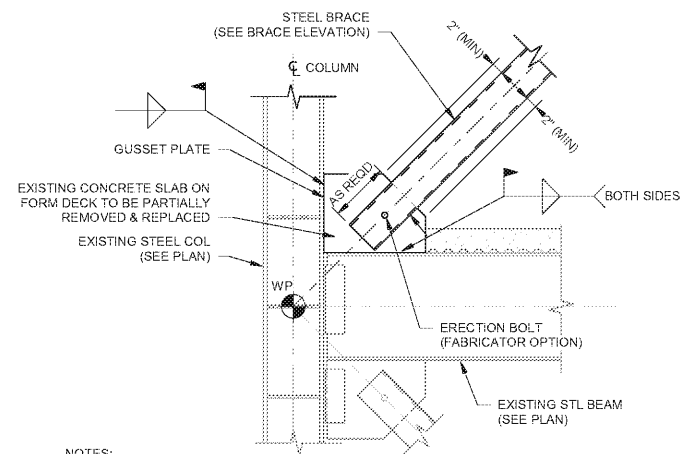
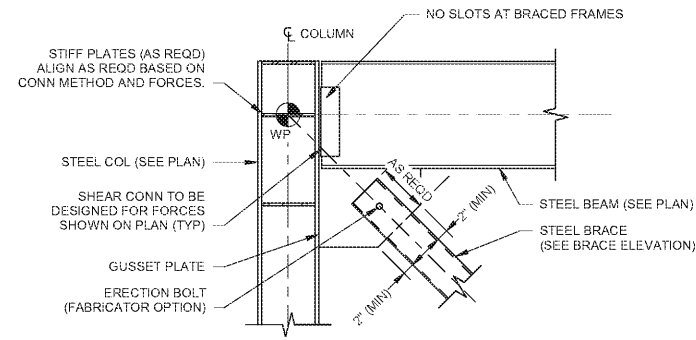
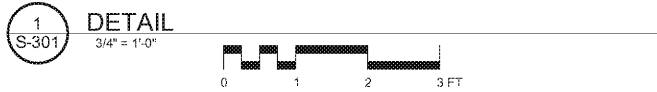


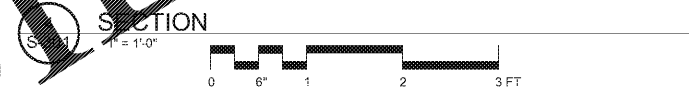
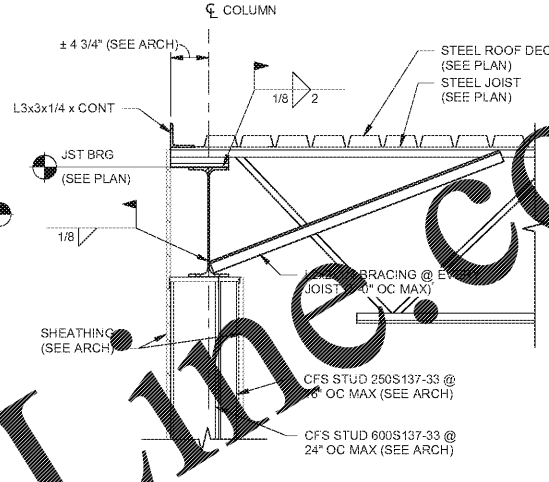
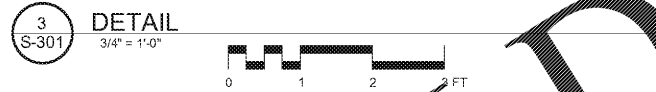
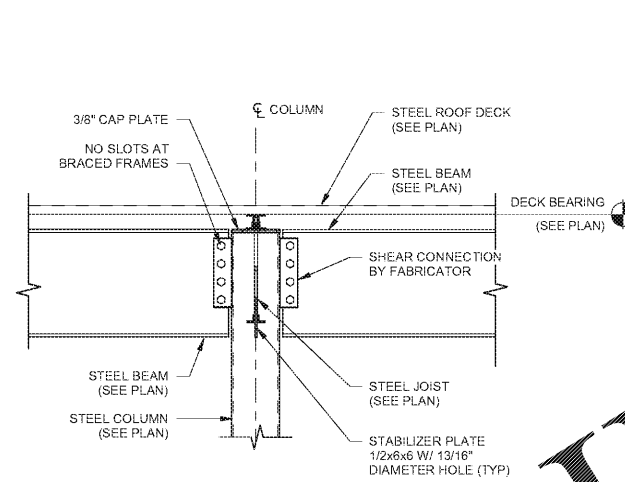
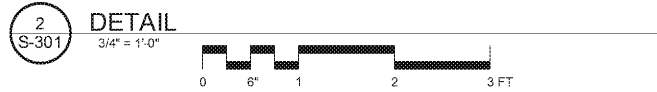
REV.	DATE BY	DESCRIPTION
B	2017-12-27	65% SUBMITTAL
C	2018-02-21	65% SUBMITTAL
D	2018-04-13	FOR CONSTRUCTION
E	2018-06-01	FOR CONSTRUCTION



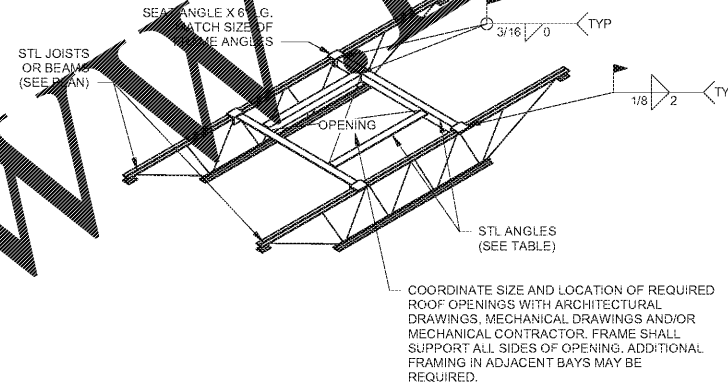
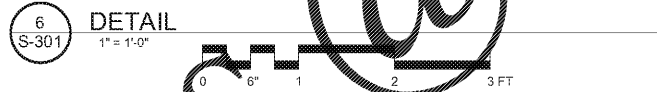
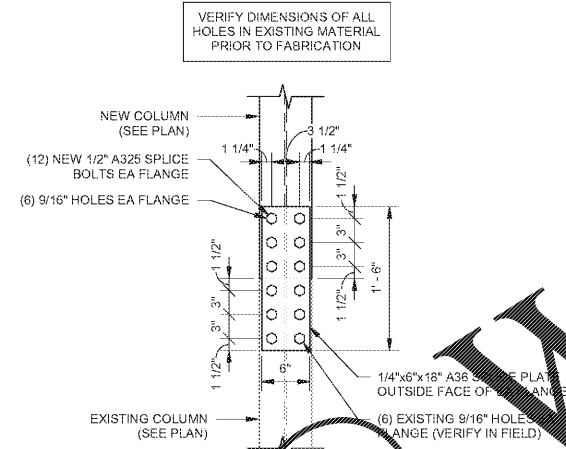
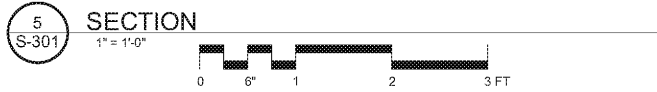
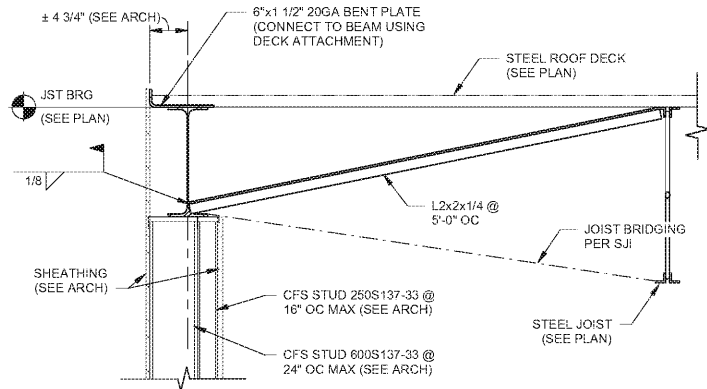
- NOTES:
- FABRICATOR SHALL SUBMIT BRACE CONNECTION CALCULATIONS WITH SHOP DRAWINGS. CONNECTIONS SHALL BE DESIGNED FOR FORCES SHOWN ON BRACE ELEVATIONS AND FRAMING PLANS.
 - FABRICATOR HAS THE OPTION TO USE BOLTED GUSSET CONNECTIONS. SUBMIT DETAIL FOR REVIEW AND ACCEPTANCE PRIOR TO SUBMITTING SHOP DRAWINGS.
 - AT SIMILAR CONDITIONS EITHER THE TOP OR BOTTOM BRACE WILL NOT BE PRESENT, AND/OR THE CONNECTION IS TO THE MINOR AXIS OF THE COLUMN.



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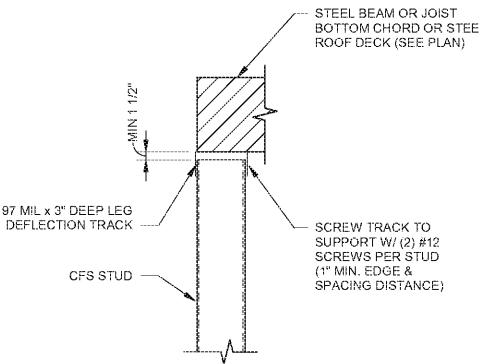
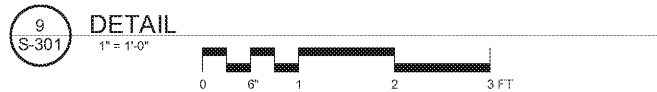
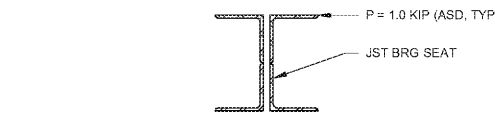
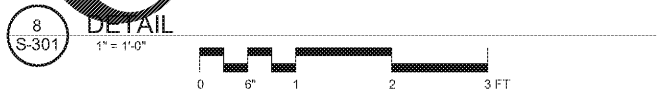
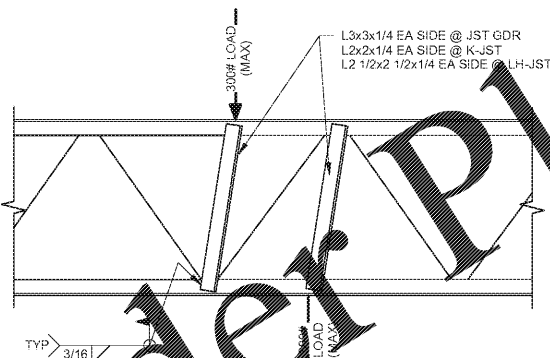


SPAN	ANGLE SIZE
UP TO 4'-0"	L3X3X3/16
4'-1" TO 6'-0"	L4X3X1/4 (LLV)
6'-1" TO 8'-0"	L5X3X1/4 (LLV)
8'-1" TO 10'-0"	L6X4X5/16 (LLV)

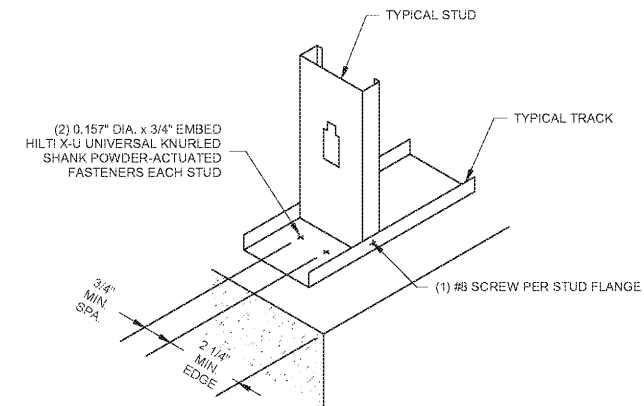


7 TYPICAL ROOF OPENING DECK SUPPORT FRAMING
NOT TO SCALE

- NOTES:
- PROVIDE SIMILAR FRAMING AT ROOF DRAINS.
 - WELD/CONNECT DECK TO ALL FRAMING AT 6" OC MAXIMUM SPACING. COORDINATE WITH COR FOR SPECIAL DETAILS REQUIRED TO CONNECT ANGLES TO EXISTING DECK IN AN INSTALLATION OF NEW UNIT ON EXISTING ROOF FRAMING.
 - WHEN JOIST BRIDGING CONFLICTS WITH ROOF OPENING FRAMES, STOP BRIDGING AT EACH SIDE OF OPENING. PROVIDE CROSS BRIDGING AT LAST BRIDGING SPACE EACH SIDE OF OPENING AND CONNECT ENDS OF BRIDGING TO OPENING FRAMING. ADD ADDITIONAL BRIDGING AND CROSS BRIDGING ON EACH SIDE OF OPENING ON EACH SIDE OF CUT BRIDGING AREA WITH BRIDGING EXTENDED ONE BAY BEYOND EACH SIDE OF OPENING.
 - ANCHOR EQUIPMENT AND CURB TO MISCELLANEOUS FRAMING SHOWN AS REQUIRED FOR WIND AND/OR SEISMIC FORCES. COORDINATE WITH EQUIPMENT/CURB MANUFACTURER (CONNECTION DESIGN NOT BY PES STRUCTURAL ENGINEERS).
 - THIS DETAIL IS NOT APPLICABLE TO ROOFTOP EQUIPMENT CURBS. REFER TO EQUIPMENT CURB SUPPORT DETAIL FOR CURB SUPPORT FRAMING.



10 TYPICAL TOP TRACK CONNECTION
NOT TO SCALE



11 TYPICAL BOTTOM TRACK CONNECTION
NOT TO SCALE

REPAIR FACILITY & ADDITION
BUILDING 747
FGWB 08-0027
DOBBINS AIR RESERVE BASE, GEORGIA

SECTIONS & DETAILS

PROJECT CONTACT:
ROBERT L. MORGAN
rlm@pescorp.com

PROJECT	LOCATION	DRAWING	CONTACT
PROJECT No.	2017-238		
PROJ. OFFICER	J. MORGAN		
DESIGNED	M. WILSON		
CHECKED	M. PLANNER		
DATE	2018-06-01		
DRAWING	8 OF 45		

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