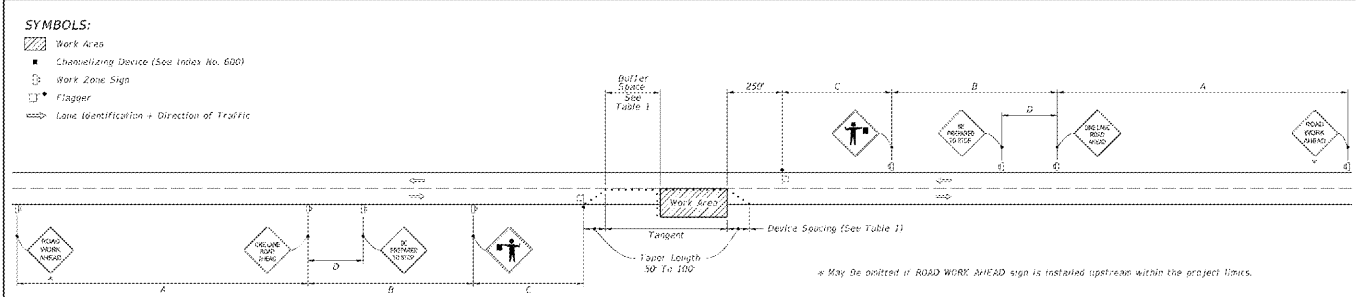


NOTE:
THIS PLAN IS PART OF AN OVERALL SET OF PLANS. THIS SHEET IS CONSIDERED INVALID UNLESS ACCOMPANIED BY ALL OTHER DRAWINGS IN THE SET OF PLANS ON THE JOB SITE OR FOR BIDDING PURPOSES. THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING A COMPLETE SET OF PLANS TO ALL THOSE CONDUCTING WORK ON THIS PROJECT.



WITHOUT TEMPORARY RAISED RUMBLE STRIPS

GENERAL NOTES:

- Special Conditions may be required in accordance with these notes and the following sheets:
 - At all active railroad crossings, if located closer to the Work Area than the queue length plus 200 Feet, extend the Buffer Space as shown on Sheet 2.
 - If the spacing of vehicles across an active railroad crossing cannot be avoided, provide a railroad traffic control officer or flagger at the highway-rail grade crossing to prevent vehicles from stopping within the highway-rail grade crossing, even if advanced traffic warning devices are in place.
- If the Work Area encroaches on the Centerline, use the Layout for Temporary Lane Shift to Shoulder or Shoulder to Lane of the Existing Paved Shoulder with its sufficient to provide for an IT Lane between the Work Area and the edge of Existing Paved Shoulder. Revert the posted speed when appropriate.
 - Use when both of the following conditions are met concurrently:
 - Existing Posted Speed is 35 mph or greater.
 - Work duration is greater than 60 minutes.
 - Use a consistent Strip color throughout the work zone.
 - Place each Rumble Strip bed transversely across the lane at four-foot intervals.
 - Use Option 1 or Option 2 as shown on Sheet 2. Use only one option throughout work zone.
- Additional one-way control may be provided by the following means:
 - Flagger or sign.
 - Official vehicle.
 - Police vehicles.
 - Traffic signals.

When flaggers are the sole means of one-way control, the flaggers must be in sight of each other or in direct communication at all times.
- When a side road intersects the highway within the TTC zone, place additional TTC devices in accordance with other applicable TC2 indexes.
 - The two channelizing devices directly in front of the work area may be omitted provided vehicles in the work area have high-intensity rotating flashing, oscillating, or strobe light operation.
 - When Buffer Space cannot be obtained due to geometric constraints, use the greatest attainable length, not less than 200 ft. For posted speeds greater than 25 mph.
 - ROAD WORK AHEAD and the BE PREPARED TO STOP sign may be omitted if all of the following conditions are met:
 - Work operations are 60 minutes or less.
 - Speed limit is 45 mph or less.
 - There are no sight obstructions to vehicles approaching the work area for a distance equal to the Buffer Space shown in Table 1.
 - Vehicles in the work area have high-intensity, rotating, flashing, oscillating, or strobe light operation.
 - Visibility and complexity of the roadway has been considered.
 - If a railroad crossing is present, vehicles will not queue across rail tracks.
 - AFADs are not to used.
 - See Index 600 for general TC2 requirements and additional information.
 - Advanced Flagger Assistance Devices (AFADs) may be used in accordance with specifications section 102.600 and the site vendor drawings.

TABLE 1
DEVICE SPACING

Posted Speed	Maximum Spacing of Cones or Tabular Markers		Maximum Spacing of Type I or Type II Barricades/Panels/Drums		Distance Between Signs				Buffer Space
	On a Taper	On a Tangent	On a Taper	On a Tangent	A	B	C	D	
25	20'	30'	20'	30'	200'	200'	200'	100'	150'
30	20'	30'	20'	30'	200'	200'	200'	110'	200'
35	20'	30'	20'	30'	200'	200'	200'	100'	250'
40	20'	30'	20'	30'	200'	200'	200'	100'	300'
45	20'	30'	20'	30'	200'	200'	200'	110'	350'
50	20'	30'	20'	30'	200'	200'	200'	120'	425'
55	20'	30'	20'	30'	200'	200'	200'	130'	500'
60	20'	30'	20'	30'	200'	200'	200'	140'	575'
65	20'	30'	20'	30'	200'	200'	200'	150'	650'
70	20'	30'	20'	30'	200'	200'	200'	160'	725'

CONDITIONS
WHERE ANY VEHICLE, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCRoACH THE AREA BETWEEN THE CENTERLINE AND A LINE 2' OUTSIDE THE EDGE OF TRAVEL WAY.

LAST REVISION: 01/01/16	DESCRIPTION: FY 2017-18 DESIGN STANDARDS	INDEX NO.: 694	SHEET NO.: 1 of 3
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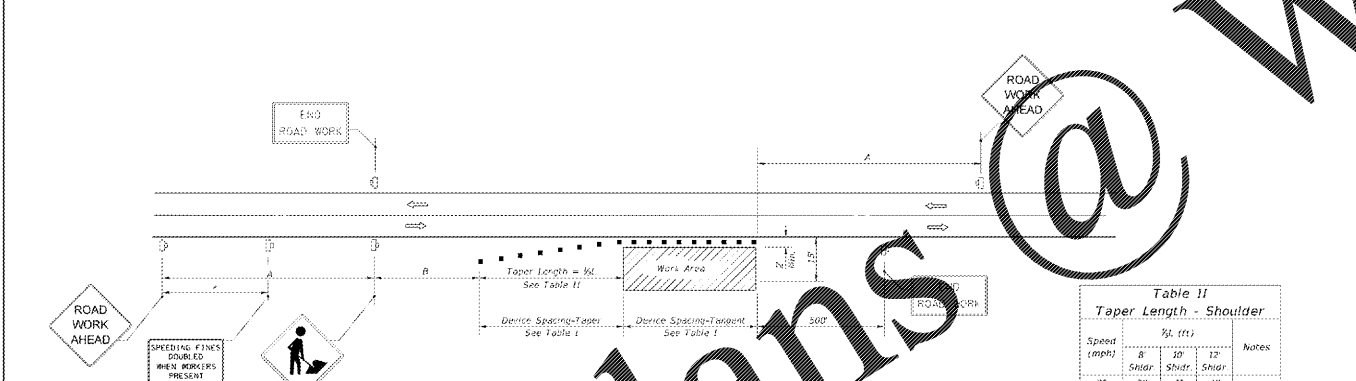
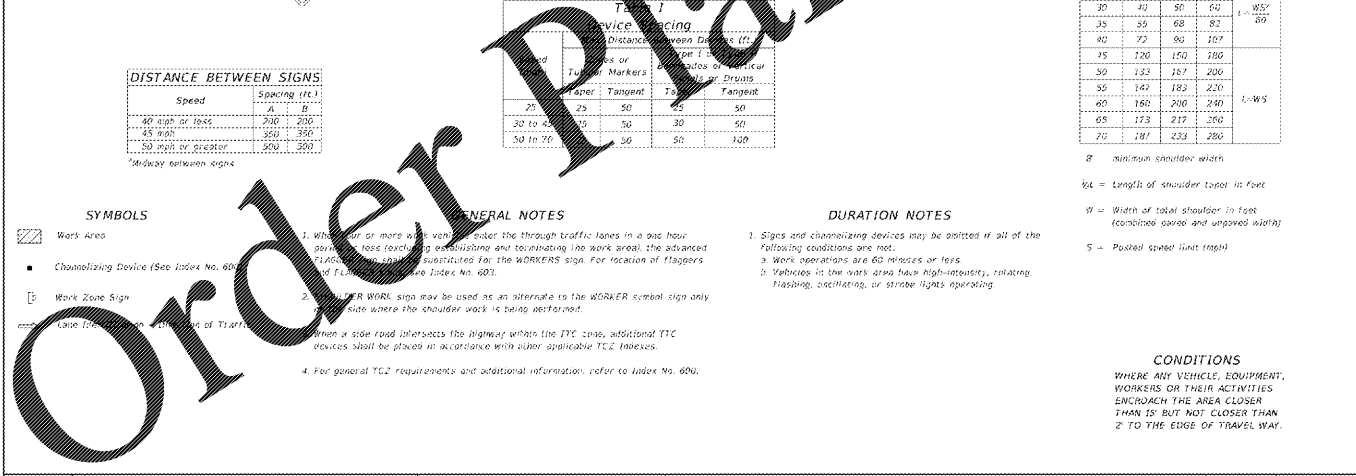


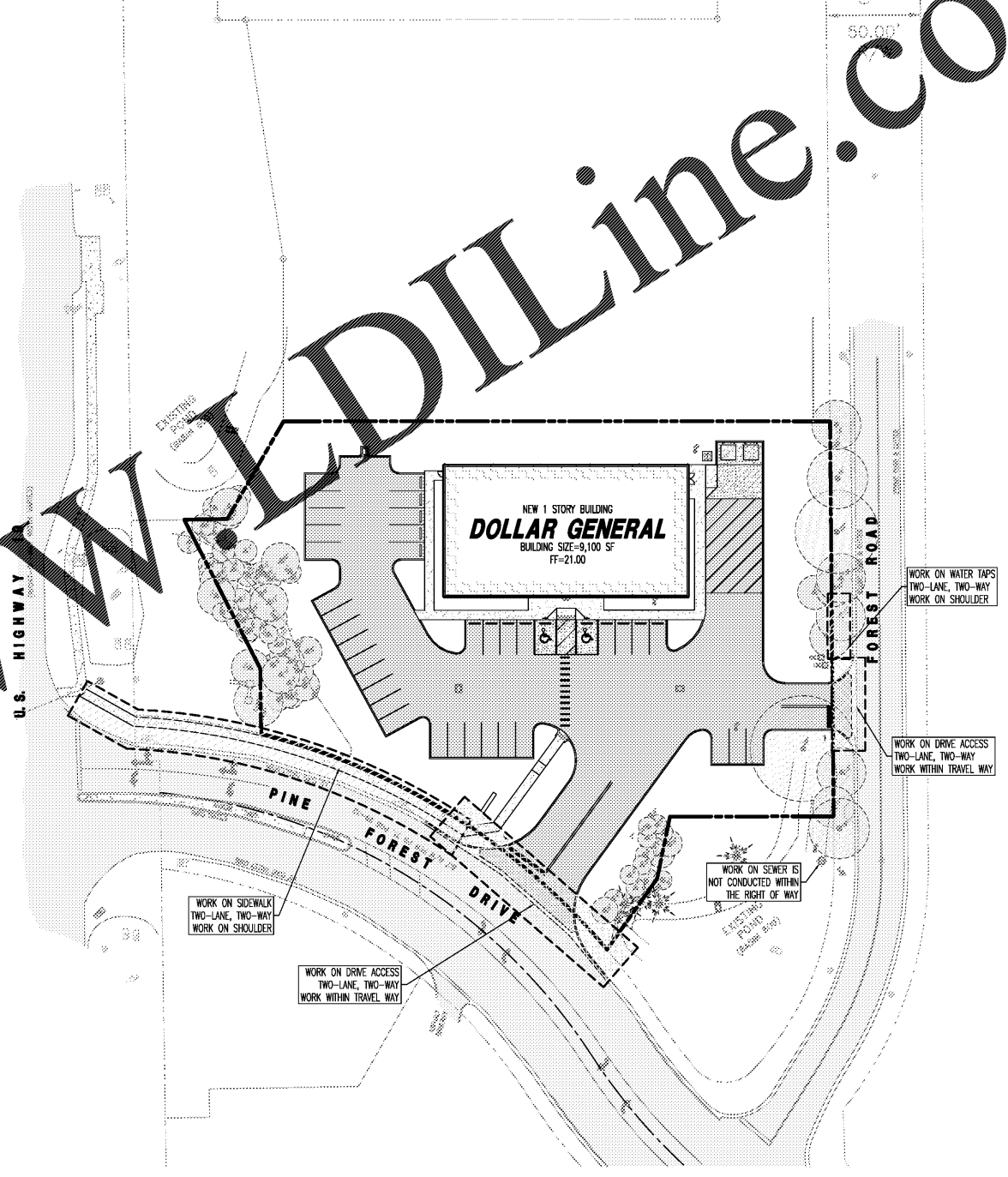
Table II
Taper Length - Shoulder

Speed (mph)	% (ft)		Notes
	Shoulder	Shoulder	
25	26	32	MS-609
30	30	37	
35	35	43	MS-609
40	40	50	
45	45	57	MS-609
50	50	64	
55	55	71	MS-609
60	60	79	
65	65	87	MS-609
70	70	95	

8' = minimum shoulder width
 50' = length of shoulder taper in feet
 W = width of total shoulder in feet (combined paved and unpaved width)
 S = posted speed limit (mph)

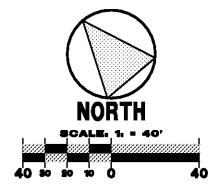


LAST REVISION: 07/01/15	DESCRIPTION: FY 2017-18 DESIGN STANDARDS	INDEX NO.: 602	SHEET NO.: 1 of 1
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MAINTENANCE OF TRAFFIC IMPORTANT NOTES TO CONTRACTOR:

- MAINTENANCE OF TRAFFIC (VEHICLE & PEDESTRIAN) SHALL CONFORM TO ALL APPLICABLE REQUIREMENTS OF HERNANDO COUNTY AND FDOT STANDARD 600 SERIES INDICES (SPECIFICALLY 601, 602, 603 & 660).
- LANE CLOSURES SHALL ONLY OCCUR BETWEEN 9:00 AM AND 3:00 PM UNLESS LANE CLOSURE CALCULATIONS ARE PROVIDED AND APPROVAL IS GRANTED BY THE HERNANDO COUNTY ENGINEER. NO NIGHTTIME MOT OR CONSTRUCTION IS ALLOWED UNLESS APPROVED BY THE HERNANDO COUNTY ENGINEER.
- SIGNS MUST BE POLE MOUNTED AND PLACEMENT OF SIGN PANELS ON BARRICADES WILL NOT BE ALLOWED.
- AN MOT PLAN SHALL BE PREPARED BY AN INDIVIDUAL WHO IS CERTIFIED BY FDOT TO PROPERLY INSTALL AND MAINTAIN MOT ELEMENTS DURING AND THROUGHOUT PROJECT CONSTRUCTION. THE MOT PLAN SHALL BE APPROVED BY HERNANDO COUNTY PRIOR TO RIGHT OF WAY PERMIT ISSUANCE.
- MOT SHALL BE INSTALLED AND MONITORED BY AN INDIVIDUAL WHO IS CERTIFIED BY FDOT TO PROPERLY INSTALL AND MAINTAIN MOT ELEMENTS DURING AND THROUGHOUT PROJECT CONSTRUCTION.



Revisions

No.	Date	By
1	02/12/18	AVF
2		
3		
4		

CADD Tech: AVF
 Designer: AVF
 Engineer: SAL

Project No: -
 Scale: -
 Date: 10/21/17

PALMETTO CAPITAL GROUP, INC.

L.A. Civil, Inc.
 Certificate of Authorization # 00029995
 PO BOX 7649
 Clearwater, Florida 33758-7649
 Phone: 727-446-9000

DOLLAR GENERAL
 SEC US 19 AND PINE FOREST DRIVE
 HERNANDO COUNTY, FL

MAINTENANCE OF TRAFFIC PLAN

PROJECT TITLE
 SHEET TITLE

SHEET No. **MT.1**