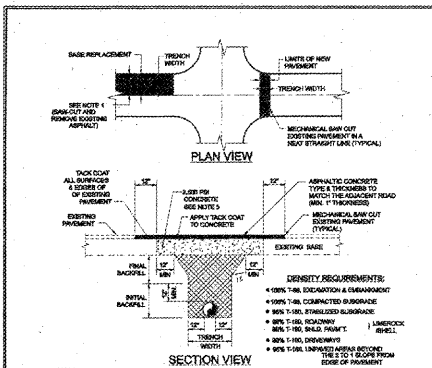


NOTES:

- 1) ALL DRIVEWAYS SHALL BE REPLACED WITH MATERIAL OF THE SAME TYPE AND COMPOSITION, AS THE MATERIAL REMOVED, TO THE LIMITS OF THE EXISTING DRIVE.
- 2) ALL CONCRETE DRIVEWAYS SHALL BE 6" THICK, 6" x 6", 10x10 GAGE WIRE MESH, CLASS 1 3000 PSI CONCRETE. THE SUB-GRADE FOR THE CONCRETE SHALL BE 6" SAND OR OTHER APPROVED MATERIAL COMPACTED TO 98% AASHTO MAXIMUM DENSITY.
- 3) ALL ASPHALT DRIVEWAYS SHALL BE 1" TYPE S-1 AND 6" COMPACTED LIMEROCK BASE.
- 4) SAW-CUT AND REMOVE ALL EXISTING DRIVEWAYS TO LIMITS AND REPLACE TO SAME GRADE AND LINES AS ORIGINAL. CONCRETE DRIVEWAYS SHOULD BE REMOVED TO THE NEAREST EXISTING JOINT.
- 5) SOD 5' MINIMUM OUTSIDE OF EDGE OF REPLACED DRIVEWAY AND ALL DISTURBED AREAS.
- 6) ROCK/SHELL AND DIRT DRIVEWAYS SHALL BE COMPACTED TO 98% MAXIMUM DENSITY PER AASHTO T-99. ROCK/SHELL DEPTH SHALL BE SIX INCHES, COMPACTED DEPTH.
- 7) CONTRACTOR SHALL REPLACE ALL DRIVEWAYS IN KIND, IN ACCORDANCE WITH THIS DETAIL, THE SPECIFICATIONS OR THE ST. LUCIE COUNTY STANDARDS, WHICHEVER IS THE MOST STRINGENT.
- 8) NEW DRIVEWAYS SHALL BE SLOPED IN A WAY THAT WILL NOT ALLOW PONDING OF STORM WATER.
- 9) CONTRACTOR SHALL DISPOSE OF ALL REMOVED MATERIAL IN A SUITABLE FASHION IN ACCORDANCE WITH ALL CITY, COUNTY AND STATE REGULATIONS.
- 10) FOR ASPHALT DRIVEWAYS, THE BASE SHALL EXTEND SIX INCHES BEYOND THE ASPHALT OVERLAY.
- 11) ALL REPLACEMENT DRIVEWAYS SHALL BE CONSTRUCTED WITH A FOOTING PARALLEL TO THE ROADWAY. FOOTING SHALL BE A MINIMUM 6" DEEP AND 8" WIDE FOR THE ENTIRE LENGTH OF THE DRIVEWAY FRONTAGE.
- 12) ALL ASPHALT DRIVEWAYS SHALL BE OVERLAYED WITHIN THE LIMITS OF THE RIGHT-OF-WAY.

TYPICAL DRIVEWAY RESTORATION
N.T.S.

	DRIVEWAY RESTORATION				R-1	NOTES/REVISIONS/ASSOCIATIONS FT. PIERCE UTILITIES AUTHORITY
	DATE	REVISION	BY	APP'D.		
DESIGNED	JAN 2010	REVISION	NO. 0000000000	DATE		
DRAWN BY	JAN 2010	NO. 0000000000	DATE			
CHECKED	JAN 2010	DATE				
APPROVED	JAN 2010	DATE				



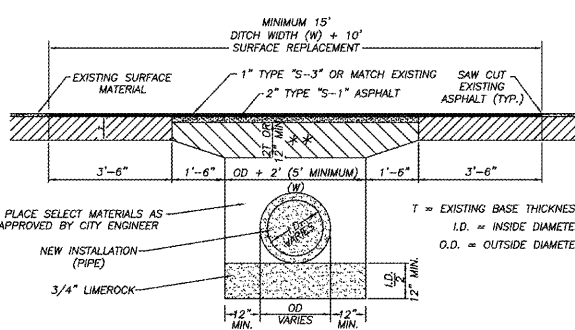
- DENSITY REQUIREMENTS:
- 1. 4 INCHES THICK EXISTING BASE AND UNDERLAYMENT SHALL BE COMPACTED TO 98% MAXIMUM DENSITY PER AASHTO T-99.
 - 2. 1/2 INCH THICK EXISTING ASPHALT SHALL BE COMPACTED TO 98% MAXIMUM DENSITY PER AASHTO T-99.
 - 3. 1/2 INCH THICK EXISTING SAND OR GRAVEL SHALL BE COMPACTED TO 98% MAXIMUM DENSITY PER AASHTO T-99.
 - 4. 1/2 INCH THICK EXISTING LIMEROCK SHALL BE COMPACTED TO 98% MAXIMUM DENSITY PER AASHTO T-99.
 - 5. 1/2 INCH THICK EXISTING ROCK/SHELL SHALL BE COMPACTED TO 98% MAXIMUM DENSITY PER AASHTO T-99.
 - 6. 1/2 INCH THICK EXISTING DIRT SHALL BE COMPACTED TO 98% MAXIMUM DENSITY PER AASHTO T-99.

NOTES:

1. EXISTING DRIVEWAYS SHALL BE REPLACED WITH MATERIAL OF THE SAME TYPE AND COMPOSITION, AS THE MATERIAL REMOVED, TO THE LIMITS OF THE EXISTING DRIVE.
2. ALL CONCRETE DRIVEWAYS SHALL BE 6" THICK, 6" x 6", 10x10 GAGE WIRE MESH, CLASS 1 3000 PSI CONCRETE. THE SUB-GRADE FOR THE CONCRETE SHALL BE 6" SAND OR OTHER APPROVED MATERIAL COMPACTED TO 98% AASHTO MAXIMUM DENSITY.
3. ALL ASPHALT DRIVEWAYS SHALL BE 1" TYPE S-1 AND 6" COMPACTED LIMEROCK BASE.
4. SAW-CUT AND REMOVE ALL EXISTING DRIVEWAYS TO LIMITS AND REPLACE TO SAME GRADE AND LINES AS ORIGINAL. CONCRETE DRIVEWAYS SHOULD BE REMOVED TO THE NEAREST EXISTING JOINT.
5. SOD 5' MINIMUM OUTSIDE OF EDGE OF REPLACED DRIVEWAY AND ALL DISTURBED AREAS.
6. ROCK/SHELL AND DIRT DRIVEWAYS SHALL BE COMPACTED TO 98% MAXIMUM DENSITY PER AASHTO T-99. ROCK/SHELL DEPTH SHALL BE SIX INCHES, COMPACTED DEPTH.
7. CONTRACTOR SHALL REPLACE ALL DRIVEWAYS IN KIND, IN ACCORDANCE WITH THIS DETAIL, THE SPECIFICATIONS OR THE ST. LUCIE COUNTY STANDARDS, WHICHEVER IS THE MOST STRINGENT.
8. NEW DRIVEWAYS SHALL BE SLOPED IN A WAY THAT WILL NOT ALLOW PONDING OF STORM WATER.
9. CONTRACTOR SHALL DISPOSE OF ALL REMOVED MATERIAL IN A SUITABLE FASHION IN ACCORDANCE WITH ALL CITY, COUNTY AND STATE REGULATIONS.
- 10) FOR ASPHALT DRIVEWAYS, THE BASE SHALL EXTEND SIX INCHES BEYOND THE ASPHALT OVERLAY.
- 11) ALL REPLACEMENT DRIVEWAYS SHALL BE CONSTRUCTED WITH A FOOTING PARALLEL TO THE ROADWAY. FOOTING SHALL BE A MINIMUM 6" DEEP AND 8" WIDE FOR THE ENTIRE LENGTH OF THE DRIVEWAY FRONTAGE.
- 12) ALL ASPHALT DRIVEWAYS SHALL BE OVERLAYED WITHIN THE LIMITS OF THE RIGHT-OF-WAY.

PAVEMENT RESTORATION
N.T.S.

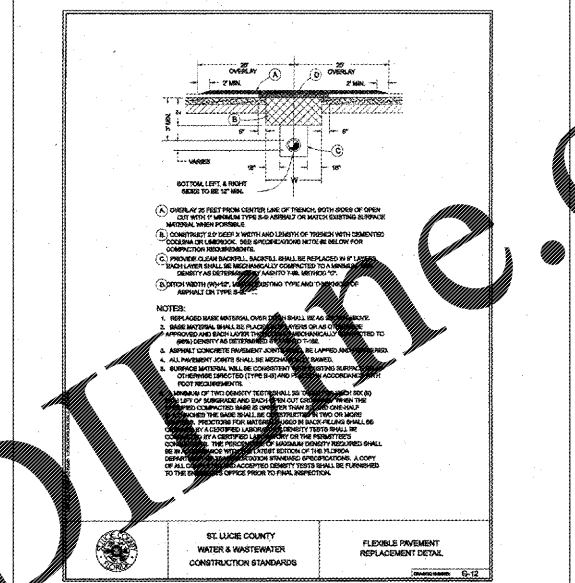
	PAVEMENT RESTORATION				R-2	NOTES/REVISIONS/ASSOCIATIONS FT. PIERCE UTILITIES AUTHORITY
	DATE	REVISION	BY	APP'D.		
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- * PLEASE SELECT MATERIALS AS APPROVED BY CITY ENGINEER
- T = EXISTING BASE THICKNESS
I.D. = INSIDE DIAMETER
O.D. = OUTSIDE DIAMETER
- * INITIAL BACKFILL SHALL BE PLACED TO 12" ABOVE THE PIPE IN 6" LIFTS. BACKFILL SHALL BE MECHANICALLY TAMPED TO A MINIMUM OF 95% OF MAXIMUM DENSITY AS DETERMINED BY AASHTO METHOD T-99. UPPERMOST 12" BE COMPACTED TO 98% OF T-99.
- ** LIMEROCK OR CRUSHED COQUINA COMPACTED TO 98% OF MAXIMUM DENSITY AS DETERMINED BY AASHTO METHOD T-180

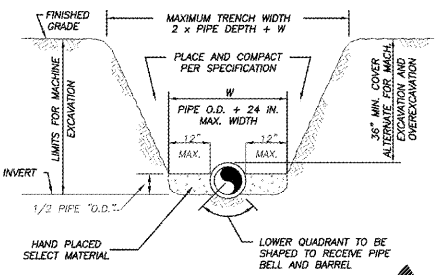
FLEXIBLE PAVEMENT RESTORATION
N.T.S.

	FLEXIBLE PAVEMENT RESTORATION				R-3	NOTES/REVISIONS/ASSOCIATIONS FT. PIERCE UTILITIES AUTHORITY
	DATE	REVISION	BY	APP'D.		
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TRENCH AND TRENCH RESTORATION
N.T.S.

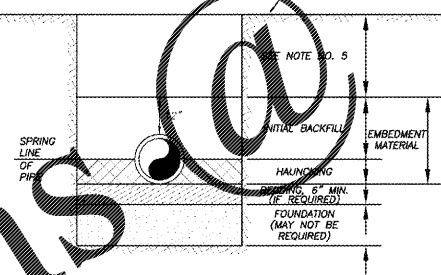
	TRENCH AND TRENCH RESTORATION				R-4	NOTES/REVISIONS/ASSOCIATIONS FT. PIERCE UTILITIES AUTHORITY
	DATE	REVISION	BY	APP'D.		
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TYPICAL TRENCH DETAILS
N.T.S.

- NOTES:
- 1) THE CONTRACTOR SHALL COMPLY WITH REQUIREMENTS OF THE FLORIDA TRENCH SAFETY ACT.
 - 2) INITIAL BACKFILL SHALL BE HAND PLACED TO 12" ABOVE THE PIPE. BACKFILL SHALL BE MECHANICALLY TAMPED TO A MINIMUM OF 100% OF MAX. DENSITY AS DETERMINED BY AASHTO METHOD T-99.

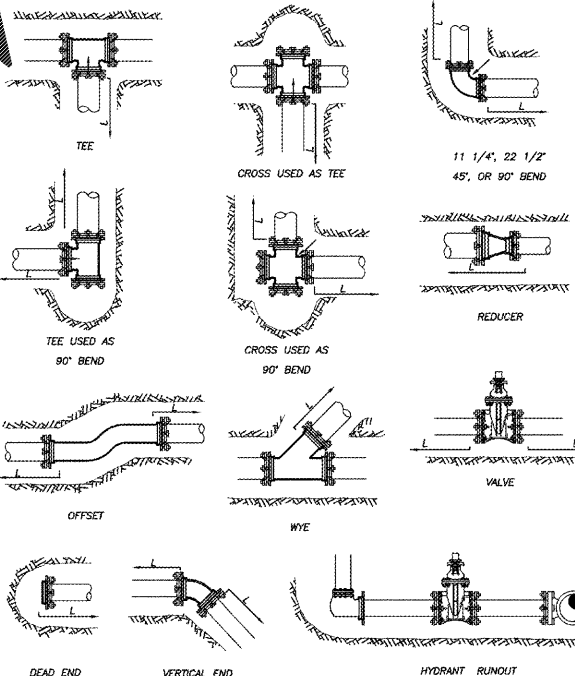
	TYPICAL TRENCH DETAILS				M-1	NOTES/REVISIONS/ASSOCIATIONS FT. PIERCE UTILITIES AUTHORITY
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BACKFILLING REQUIREMENTS
N.T.S.

- NOTES:
- 1) IN CERTAIN SOIL CONDITIONS A FOUNDATION MAY BE REQUIRED.
 - 2) BEDDING IS REQUIRED PRIMARILY TO BRING THE TRENCH BOTTOM UP TO GRADE. BEDDING MATERIALS SHALL PROVIDE A UNIFORM AND ADEQUATE LONGITUDINAL SUPPORT UNDER THE PIPE.
 - 3) HAUNCHING MATERIAL SHALL BE HAND PLACED TO THE SPRINGLINE OF THE PIPE. MATERIAL SHALL BE CONSOLIDATED UNDER THE PIPE AND TAMPED TO PROVIDE ADEQUATE SIDE SUPPORT.
 - 4) INITIAL BACKFILL MATERIAL SHALL BE HAND PLACED TO 12" ABOVE THE TOP OF PIPE. THE SOIL SHALL BE COMPACTED TO 100% MAX. DENSITY (AASHTO T-99).
 - 5) BACKFILL SHALL BE COMPACTED TO 100% OF MAX. DENSITY AS PER AASHTO T-99, TO A POINT 30" BELOW PROPOSED PROFILE GRADE OR EXISTING GRADE. THE FINAL 30" OF BACKFILL SHALL BE COMPACTED TO 98% OF MAX. DENSITY AS PER AASHTO T-180.
 - 6) DENSITY TEST SHALL BE PERFORMED AT AREAS DETERMINED BY THE UTILITIES ENGINEER OR PERMIT AGENCY HAVING JURISDICTION, AT THE CONTRACTORS EXPENSE.
 - 7) CONTRACTOR TO COMPLY WITH ALL FEDERAL, STATE AND LOCAL TRENCH SAFETY REGULATIONS.

	BACKFILLING REQUIREMENTS				M-2	NOTES/REVISIONS/ASSOCIATIONS FT. PIERCE UTILITIES AUTHORITY
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MECHANICAL JOINT ANCHORING REQUIREMENTS
N.T.S.

	MECHANICAL JOINT ANCHORING REQUIREMENTS				M-3	NOTES/REVISIONS/ASSOCIATIONS FT. PIERCE UTILITIES AUTHORITY
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MECHANICAL JOINT RESTRAINT NOTES

- NOTES:
- 1) THE ENGINEER SHALL BE RESPONSIBLE FOR DETERMINING THE REQUIRED LENGTH TO BE RESTRAINED BASED UPON THE PROJECT AREA SOIL TYPE, PROPOSED TRENCH CONDITIONS AND DEPTH, PRESSURE OF 150 PSI, AND A SAFETY FACTOR OF TWO (2). A DRAWING OF EVERY TYPICAL FITTING ASSEMBLY WITHIN THE PROJECT SHALL BE SUBMITTED WHICH REFLECTS THE RESTRAINT DETAIL PROPOSED FOR USE, INCLUDING LENGTH OF PIPE RESTRAINT.
 - 2) REQUIRED RESTRAINED LENGTH CALCULATIONS SHALL ALSO CONSIDER THE CONDITIONS OF OTHER BENDS OR FITTINGS THAT WILL BE LOCATED WITHIN THE CALCULATED RESTRAINED LENGTH (L) OF THE BEND OR FITTING IN QUESTION.
 - 3) EVERY JOINT OR FITTING MUST BE RESTRAINED ON BOTH SIDES OF THE BEND AND FOR TEES ALONG THE BEND ALSO.



MECHANICAL JOINT RESTRAINT NOTES
N.T.S.

	MECHANICAL JOINT RESTRAINT NOTES				M-4	NOTES/REVISIONS/ASSOCIATIONS FT. PIERCE UTILITIES AUTHORITY
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BOHLER ENGINEERING
 2255 GLADES ROAD, SUITE 3035
 BOCA RATON, FLORIDA 33431
 Phone: (561) 571-0280
 Fax: (561) 571-0281
 Florida Business Journal of 2009
 1000 Paces Blvd., Suite 1000, Boca Raton, FL 33431

REV	DATE	COMMENT	BY
1	8/9/2016	PER CITY COMMENTS	SMT
2	10/20/2016	REVISIONS PER SFWM COMMENTS	SMT
3	4/10/2017	PER FOOT AND SFWM COMMENTS	DJE
4	4/24/2017	PER CITY OF FORT PIERCE COMMENTS	SGM
5	5/12/2017	PER CITY OF FORT PIERCE COMMENTS	SGM
6	6/2/2017	PER FOOT COMMENTS	SGM
7	7/14/2017	PER FOOT COMMENTS	SGM

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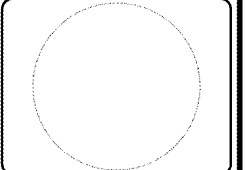
APPROVED FOR CONSTRUCTION

PROJECT NO.	FLB15009
DRAWN BY	DJE
CHECKED BY	CR
DATE	06/24/16
SCALE	
CAD I.D.	SD3

PROJECT: **SR 70 AND JENKINS FOR WAWA**

LOCATION OF SITE
 SEC SR 70 & S JENKINS RD
 FORT PIERCE, FLORIDA

BOHLER ENGINEERING
 2255 GLADES ROAD, SUITE 3035
 BOCA RATON, FLORIDA 33431
 Phone: (561) 571-0280
 Fax: (561) 571-0281
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 1000 Paces Blvd., Suite 1000, Boca Raton, FL 33431



SHEET TITLE: **CONSTRUCTION DETAILS**

SHEET NUMBER: **D-5**